

10 VILLAGE CENTER PLAN

10.1 INTRODUCTION

The Village Center is envisioned as the heart and primary focus of the West Roseville Specific Plan. Encompassing an area of approximately 120.78 acres, the Village Center provides for a true interactive mixed-use neighborhood. Retail, office, community services, public spaces and parks are integrated with a variety of housing types. The mix, density, and interface between these uses create an environment that is pedestrian friendly and where formal and informal activities are key goals.

The Village Center presents a unique development form in Roseville, one emblematic of an urban mixed-use environment. It introduces concepts, product types, use patterns, interfaces and development standards that require adjustment to the general philosophies and regulations that typically guide suburban development. Successful execution of the Village Center is dependent upon a clear articulation of, and a long-term commitment to, the base concepts and principles that define its distinctive form. While it is critical that policies and regulations be developed that are firm in ensuring implementation of the Village Center's distinguishing components, it is similarly important to provide sufficient flexibility to accommodate the variety of specific product types and design solutions that could contribute towards its realization.

Successful execution of the Village Center is dependent upon a clear articulation of, and a long-term commitment to, the base concepts and principles that define its distinctive form.



Given the unique nature of the Village Center, and the important role it plays in the WRSP, detailed requirements have been established to guide its development. This section of the Specific Plan provides specific form, planning principles, land use, zoning, development and design standards and processing direction. It supplements other sections of the Specific Plan and includes a more specific level of detail.

10.2 VILLAGE CENTER FORM & PLANNING PRINCIPLES

□ FORM

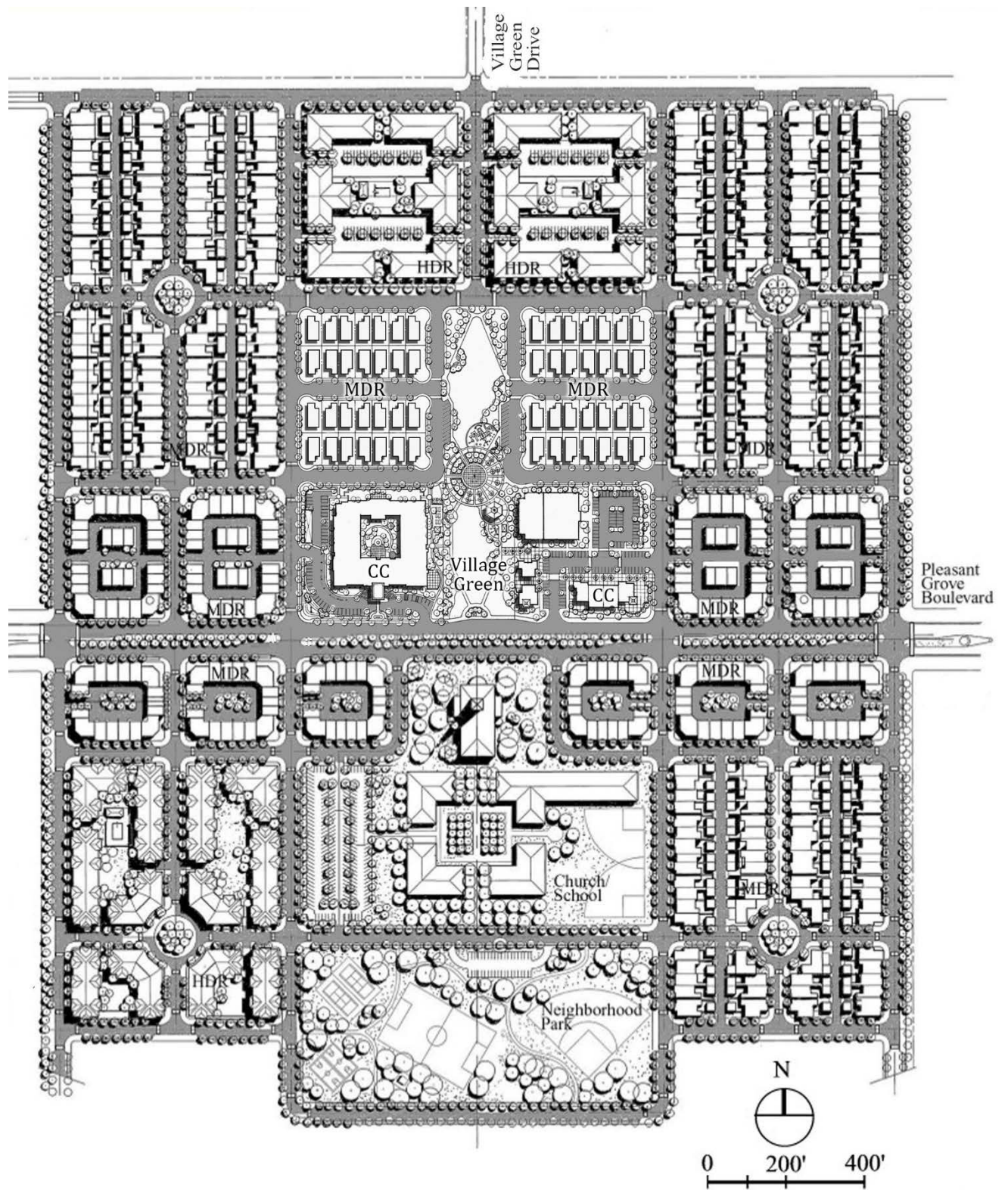
The Village Center is a core feature and a unique place within the WRSP. It is influenced by the concepts embedded in traditional small town urban villages and smart growth principles such as densities that support transit, a mix of uses, compact development form, a variety of housing opportunities and a pedestrian-friendly environment. The Village Center incorporates a broad mix of uses applied over an urban grid street pattern with a central village green. Included are a blend of higher density residential types, commercial and service uses, and public/quasi-public spaces. The increased density/intensity of development and spatial relationship between uses promote diversity, activity and pedestrian orientation. Numerous design elements, including buildings oriented toward the street and dense tree canopies, are incorporated to further this intent. The Village Center form and density facilitates a variety of transportation options and increases the living choices available in Roseville.



The Village Center is planned to function as the commercial, service, social and activity focus of the WRSP. The Village Center will have a visible presence in the community and create a destination where residents may live, shop, eat, recreate and obtain services. It is the gathering spot and a district where preference is given to the pedestrian. Interaction and socialization are primary goals and a catalyst for formal and informal activities. Uses, activities and amenities are offered that encourage people from throughout the community to come often and stay for extended periods.

The key elements of the Village Center are generally reflected on Figure 10-1, Village Center Illustrative

Figure 10-1: Village Center Illustrative Plan



□ **PLANNING PRINCIPLES**

The Village Center is defined by a combination of its form, uses and design. The following principles describe the major attributes of the use and guide its implementation. The Village Center Planning Principles are highly influenced by and reflective of “Smart Growth” values that promote the creation of walkable mixed-use communities:

Form:

- Model the Village Center in the form of a traditional small town village or center;
- Create a clearly identifiable and distinct district that is easy to see, find, get to and comprehend;
- Integrate a mix of uses with multiple destinations within close proximity;
- Increase densities and intensities of uses to support activity and a variety of transportation choices;
- Create a compact development form;
- Provide overlapping interfaces between uses and allow uses to mix;
- Create a pedestrian friendly-walkable community that minimizes barriers, emphasizes the pedestrian over the automobile, and implements the City’s General Plan Pedestrian District policy;
- Incorporate a traditional grid circulation pattern with short blocks, straight streets and intersections at regular intervals;
- Provide for easy and desirable access between uses, and interconnection with adjacent neighborhoods;
- Ensure that streets and sidewalks balance all forms of transportation; and
- Orient all development toward the street (rather than inward or to parking lots) to place activity on the street.

Uses:

- Create a wide range of higher density housing choices such as brownstones, townhouses, stacked flats, and apartments;
- Include retail and services uses to meet resident’s needs and to attract outside users and activities;
- Promote a “live-work” environment including residential above commercial spaces;
- Incorporate distinctive civic and quasi-public spaces for people to gather and to reinforce community identity; and
- Provide a village green as a visual centerpiece and a venue for a wide variety of activities such as farmer’s markets, arts and craft shows, various events and celebrations, and performance arts.

Design:

- Emphasize urban streetscapes as a predominant design element bringing buildings and activities to the sidewalk to create a vertical enclosure that defines comfortable public spaces;
- Incorporate deciduous tree canopies, coordinated street furnishings, plazas and multi-use spaces, pedestrian scale lighting, directional signage and other elements to enhance the pedestrian environment;
- Provide gateways to create a sense of identity and arrival to the Village Center;
- Require street forward building orientations, entries, porches, windows and other elements to create visual interest and activity along the pedestrian ways;
- De-emphasize the visual presence of garages and off-street parking through rear oriented parking areas, alley access and by optimizing on-street parking to meet a portion of the parking demand; and
- Permit adequate flexibility to accommodate the variety of specific product types and design solutions that contribute to the realization of the Village Center.

The Village Center presents a distinctive development form, characterized by a grid-like street network and buildings that are oriented to streets, that may challenge certain established attitudes and conventions. As a result, it is critical to maintain a determined commitment to the components and principles that define the Village Center, with recognition of long-term goals over short-term market pressures.



10.3 LAND USE PLAN & REGULATIONS

□ OVERVIEW

The Village Center includes a mix of residential, commercial, park and public/quasi-public uses. All uses include the Village Center combining land use designation to recognize their unique nature and requirements. Village Center land uses are reflected on Figure 10-2, and are summarized on Table 10-1. A parcel-by-parcel land use, zoning, unit and square footage summary is included on Table 10-2.

Table 10-1: Village Center Land Use Summary

General Plan Designation	Land Use	Acres	% of Total Acres	Units
VC - MDR	Medium Density Residential	53.73	44%	454
VC - HDR	High Density Residential	21.75	18%	386
VC - CC	Community Commercial	6.41	5%	
VC - P/R	Park & Recreation	12.69	11%	
VC - P/QP	Public/Quasi-Public	10.83	9%	
	Road Right of Way	15.37	13%	
TOTAL		120.78	100%	840

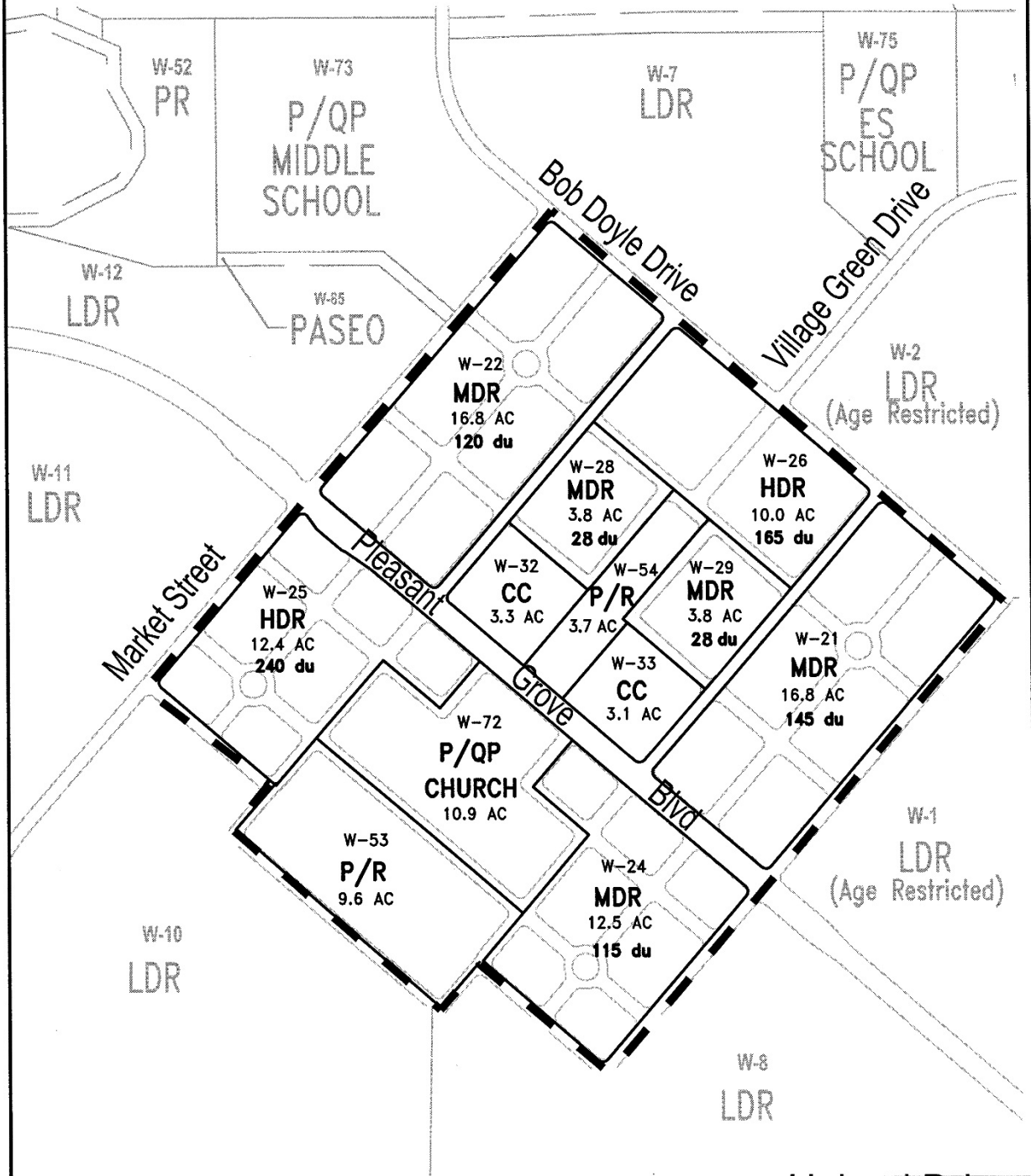
Table 10-2: Village Center Land Use, Zoning, Units & Square Footage by Parcel

Parcel #	Land Use	Zoning	Acres	Density ¹	Units ²
W-21	VC-MDR	R3/DS	16.87	8.2 du/ac	138
W-22	VC-MDR	R3/DS	16.61	8.3 du/ac	138
W-24	VC-MDR	R3/DS	12.65	9.1 du/ac	122
W-25	VC-HDR	R3/DS	12.36	19.3 du/ac	232
W-26	VC-HDR	R3/DS	9.39	16.5 du/ac	154
W-28	VC-MDR	RS/DS	3.80	7.4 du/ac	28
W-29	VC-MDR	RS/DS	3.80	7.4 du/ac	28
W-32	VC-CC	CC/SA-WR	3.31		
W-33	VC-CC	CC/SA-WR	3.10		
W-53	VC-PR	P/R	8.98		
W-54	VC-PR	P/R	3.71		
W-72	VC-P/QP	P/QP/SA-WR	10.83		
	Road Right-of-Way		15.37		
TOTAL			120.78		840

1. A mix of unit types and resulting densities are anticipated. Identified densities reflect an average as applied to the entirety of each parcel.
2. The WRSP allows for Minor Density Adjustments for LDR and MDR Units. Confirm current unit counts with the City of Roseville Planning Department.

Figure 10-2

Village Center Land Use Plan



Note: Underlying parcel basemap used in this figure is not regularly updated. Refer to Figure 4-1 for the most up to date parcel basemap.

□ PERMITTED USES AND DEVELOPMENT STANDARDS

Land uses within the Village Center are implemented through application of zone districts as specified by the City of Roseville Zoning Ordinance. In recognition of its distinctive form, the Development Standards (DS) or Special Area (SA) overlay zones have been applied to most Village Center Uses. The overlay zones customize development standards and/or permitted uses of general zone districts to reflect the unique nature of the Village Center. A summary of zoning districts and overlay zones applied to the Village Center is included in Table 10-3. Guidance relating to the application of these overlay zones is provided in this section.

The WRSP Design Guidelines (Section 12) include additional details to be considered in the design, review and approval of individual projects within the Village Center.

Table 10-3: Summary of Village Center Applied Zoning Districts

Land Use	Applied Zoning Districts	Permitted Uses	Development Standards
VC – MDR: <i>Medium Density Residential</i>	R3/DS & RS/DS	Per Zoning Ordinance	Modified by WRSP¹
VC – HDR: <i>High Density Residential</i>	R3/DS	Per Zoning Ordinance	Modified by WRSP¹
VC – CC: <i>Community Commercial</i>	CC/SA-WR	Modified by WRSP¹	Modified by WRSP¹
VC - P/R: <i>Parks & Recreation</i>	P/R	Per Zoning Ordinance	Per Zoning Ordinance
VC - P/QP: <i>Public/Quasi-Public</i>	P/QP/SA-WR	Modified by WRSP¹	Per Zoning Ordinance

1. Section 10.3

□ RESIDENTIAL USES

A wide range of higher density single-family attached, detached and multi-family housing types are included and encouraged within the Village Center. Housing is to be of an urban nature with higher densities than found in a typical suburban setting. Densities within the Village Center will range from 7.4 to over 19.3 units per acre, with an overall average density of approximately 12 units per acre. Emphasis is placed on defining the relationship between residential units and the street. Dwelling units are to be brought forward with entry doors and active living areas facing the street. Generally, off-street parking is to be de-emphasized through alley access and rear yard garages. The intent is to create an active, pedestrian friendly community environment reminiscent of a traditional neighborhood.

Medium Density Residential (VC-MDR)	
Density Range: Applied Zoning District:	7.0 to 12.9 dwelling units per acre R3/DS: Attached or Detached Housing/Design Standard Overlay RS/DS: Small Lot Residential/Development Standard Overlay
Description:	<p>Medium Density Residential (MDR) land use is anticipated to accommodate urban density housing with a strong orientation toward the street. Key components of any MDR unit type shall include:</p> <ul style="list-style-type: none"> ▪ Single family attached or detached units facing the public street ▪ Entries/porches facing and moved forward towards the street ▪ Alley loaded or recessed rear yard garages, excluding Parcels W-28 and W-29 <p>The preferred model is the “brownstone” concept reflective of the above elements. Other anticipated unit types include townhomes, and single-family detached. Figures 10-3 through 10-5a include illustrative layouts for potential unit types. There are other unit types that may be accommodated within the MDR land use and achieve the Planning Principles for the Village Center. All unit types shall be consistent with the R3/DS development standards, as well as the RS/DS development standards for parcels W-28 and W-29, and the WRSP Design Guidelines.</p> <p>Table 10-2 includes specific unit allocations and resulting densities for each MDR parcel. While it is anticipated and encouraged that unit types be mixed on a large lot parcel, the overall density in the Village Center shall average that prescribed on Table 10-2. In no case may a specific unit type on any MDR large lot parcel be below or above the density range specified by the MDR land use (7.0 to 12.9 du/ac).</p>
Permitted Uses:	As specified in the City of Roseville Zoning Ordinance.
Development Standards:	The Design Standard (DS) Overlay provides for the unique nature and variety of housing types anticipated in the Village Center and the MDR density range. Development standards are included on Table 10-4. Standards are specified for both detached and attached units. Product types, building footprints (with associated fit lists) and architectural design of units shall be approved pursuant to the Application Review Process outlined in the City’s adopted Community Design Guidelines for Compact Residential Development. Modifications to the approved architectural design of units may subsequently be approved administratively if in compliance with Table 10-4. Other product types consistent with the intent of the Village Center development standards and Design Guidelines may be considered subject to approval of a Design Review Permit for Residential Subdivisions (DRRS) concurrent with approval of a tentative subdivision map.

Table 10-4: Village Center MDR Development Standards for R3/DS

R3/DS Development Standards (MDR)			
General Characteristics			
Product Type	Detached units (Single-Family)	Attached units (Brownstone/Townhome/Condominium)	Other Products (Courtyard/"6-pak"/other deviations to standards)
Garage Access	Via rear alley or through front via 10 ft.-wide "Hollywood Drive" with a minimum 3 ft.-wide central landscape strip. See Figure 10-5 for example of "Hollywood Drive".	Via rear alley or auto court	As approved, see below.
Driveway Access	Direct access from street to garage shall be prohibited along key Village Center roadways to minimize disruption to on-street parking opportunities as specified on Figure 10-6.		
Lot Size (minimum)			
Area Interior Lot	4,000 sq. ft.	Varies	City will consider other product types and/or deviations to development standards provided they are consistent with the intent of the Village Center development standards, and provided the products demonstrate superior siting characteristics and architectural design merits.
Area Corner Lot	5,000 sq. ft.	Varies	
Width Interior Lot	40 ft.	Varies	
Width Corner Lot	50 ft.	Varies	
Permitted Density (maximum per lot)			
Residential Density	1 dwelling 1 second unit	1 dwelling	1 dwelling
Setbacks and Treatments (front setbacks measured from back of sidewalk, others from property line)			
Front Setback (Figure 10-7)	10 ft. minimum 15 ft. maximum	10 ft. minimum 15 ft. maximum	City will consider other product types and/or deviations to development standards provided they are consistent with the intent of the Village Center development standards, and provided the products demonstrate superior siting characteristics and architectural design merits.
Side	5 ft. interior; 3 ft. interior for detached garage; 12.5 ft. street side on corner	12.5 ft. street side on corner	
Rear	20 ft. (1st floor of dwelling area) 10 ft. (2nd floor) 5 ft. (garage and rear fencing from alley property line when alley access) 3 ft. (detached garage with front access)	5 ft. (minimum garage and rear fencing from alley property line.)	

R3/DS Development Standards (MDR)			
Front Off-Street Parking Setback (See Figure 10-6 for limitations on front access off-street parking)	30' minimum for garages	Garage access from alley only.	
Front Porches/ Stoops/ Bay Windows (Figure 10-7)	Open porches may project up to 6 ft. into front setback; Bay windows and other projections may project up to 3 ft. into setback. A projecting porch/ stoop may be elevated, but shall not extend more than 4 feet 6 inches above the grade of the adjacent sidewalk.	Open porches may project up to 6 ft. into front setback; Bay windows and other projections may project up to 3 ft. into setback; Steps/ stoop may project to back of sidewalk. A projecting porch/stoop may be elevated, but shall not extend more than 4 feet 6 inches above the grade of the adjacent sidewalk.	
Front Articulation	<p>The residential design intent is to achieve a design that is varied, provides visual interest, and maintains a pedestrian scale for attached and detached housing. The front street façade shall be well articulated and shall reflect the following:</p> <ul style="list-style-type: none"> ▪ <i>Wall Planes</i> – incorporating building projections and recesses, bay windows, front porches, entries, balconies, chimneys, and other elements. Two-story high walls in the same plane should be minimized and in no case shall any two-story wall comprise more than 50% of the front façade. ▪ <i>Rooflines</i> – utilizing a variety of roof forms, heights and styles. ▪ <i>Architectural Enhancements</i> – including decorative trim, shutters, columns and other articulation. ▪ <i>Finish</i> – combining multiple materials and textures to enhance architectural detail ▪ <i>Unit Variation</i> – using the above and other elements to create distinction between individual units. 		
Coverage			
Site Coverage	n/a	n/a	As approved, see above.
Height			
Height (maximum)	35 ft.	45 ft.	As approved, see above.
Parking (minimum)			
Spaces	Two (2) off-street spaces per unit. Second units do not require additional off-street parking spaces. Guest parking may be provided on-street where adequate curb length is provided through use of rear accessed garages (no additional off-street parking required).		
Development Approval			
Processing	As approved by Tentative Subdivision Map & Design Review ¹ of housing product types, footprints and architectural design. Each large-lot parcel shall be planned, processed, and approved comprehensively. Large lot parcels may not be split for purposes of development entitlement processing.		As approved with concurrent processing of Tentative Subdivision Map, Design Review for Residential Subdivision, & Design Review ¹ of housing product types and footprints.

1. Design Review approval effective until unit is occupied.

Figure 10-3: Village Center MDR (R3/DS) Brownstone-Picture and Plan View

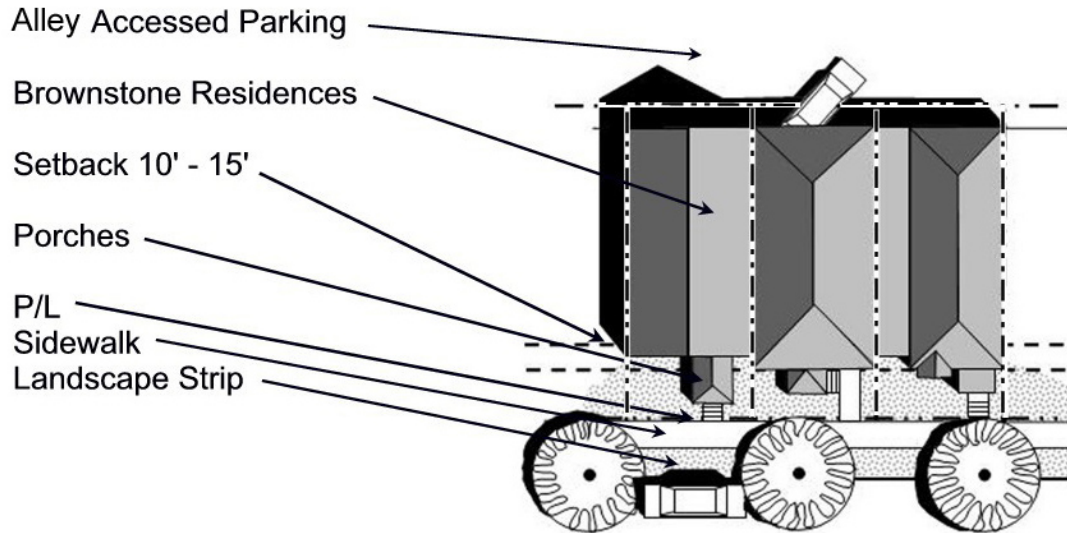


Figure 10-4: Village Center MDR (R3/DS) Townhouse-Picture and Plan View

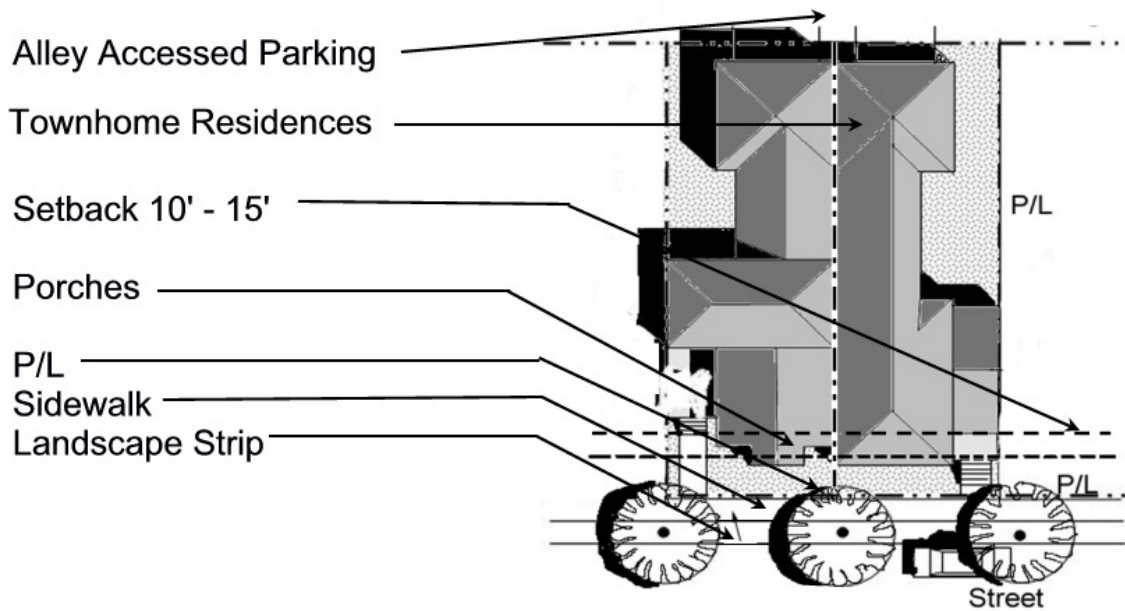
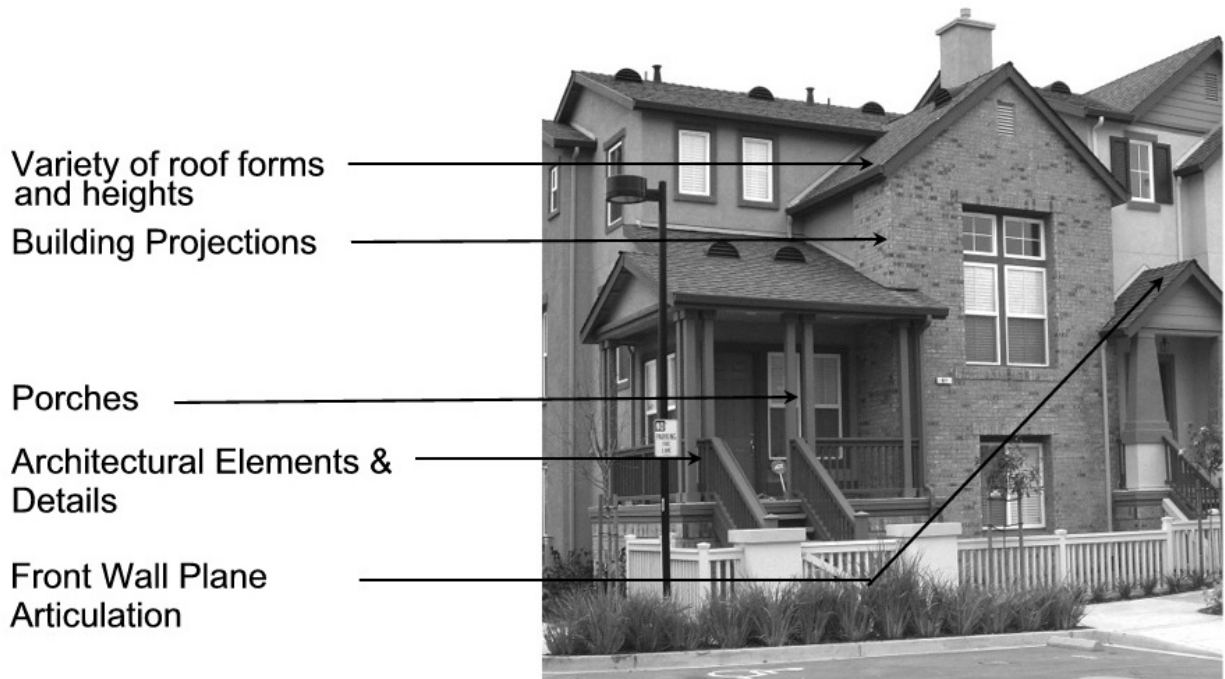


Figure 10-5: Village Center MDR (R3/DS) Single Family Detached-Picture and Plan View

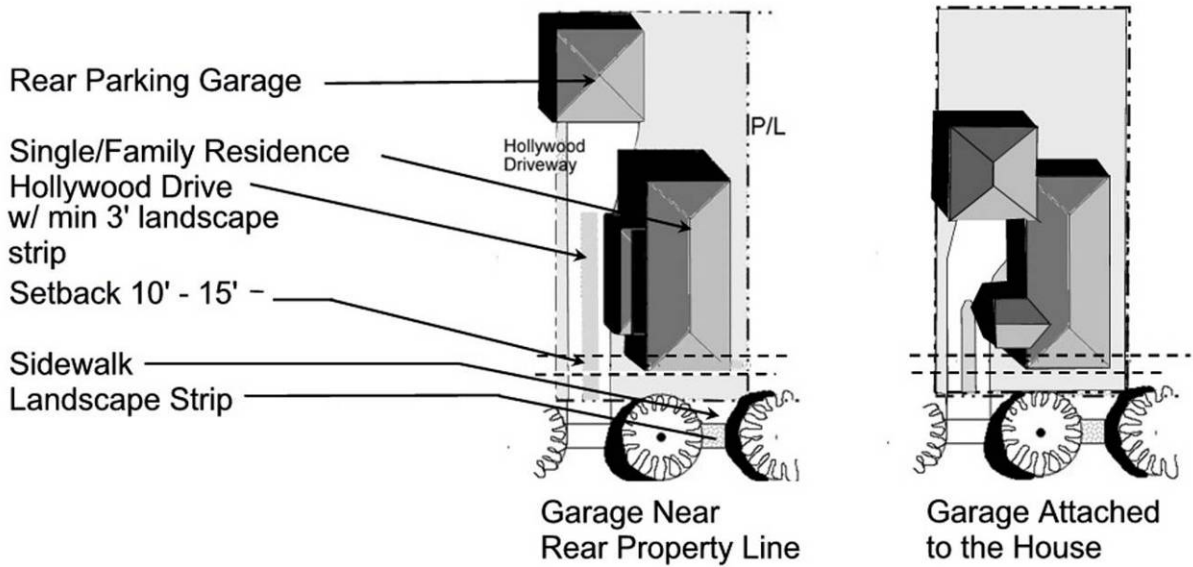
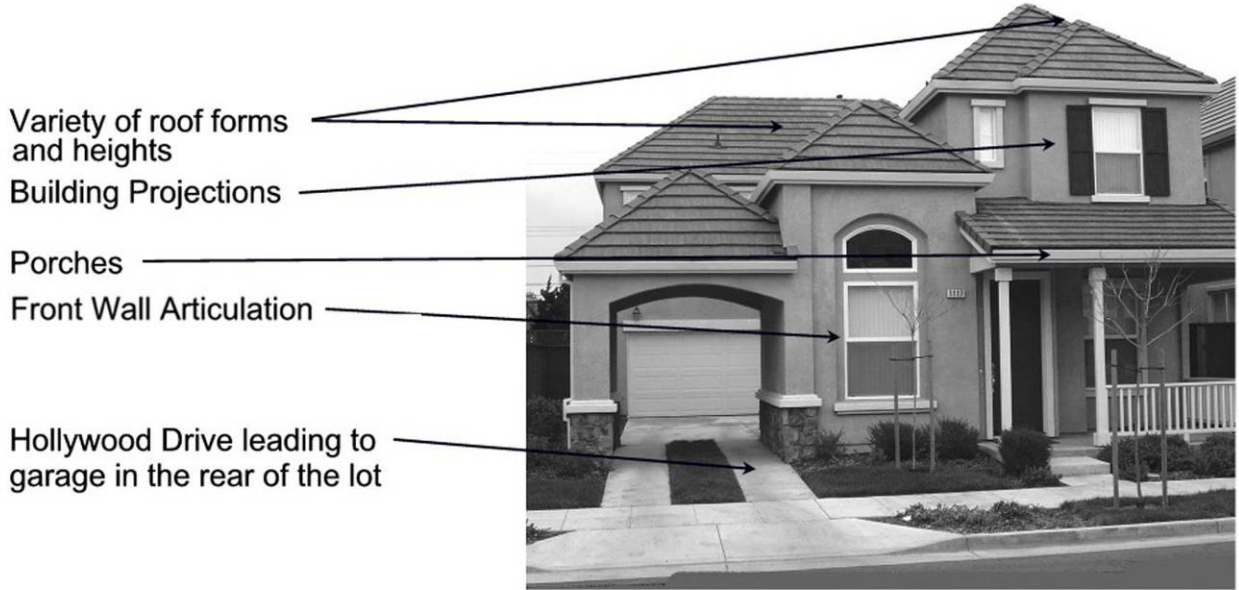


Table 10-4a: Village Center MDR Development Standards for RS/DS (Parcels W-28 & W-29)

RS/DS Development Standards (MDR)		
Product Type		
	Single Family with Attached Sidewalk ⁴	Single Family with Separated Sidewalk ^{1,4}
Lot Size (minimum)		
Area, Interior Lot	3,600 sq. ft.	3,600 sq. ft. ¹
Area, Corner Lot	4,400 sq. ft.	4,000 sq. ft. ¹
Width, Interior	45 ft.	45 ft.
Width, Corner	55 ft.	50 ft.
Permitted Density (maximum per lot)		
Residential Density	1 dwelling; 1 second unit	1 dwelling; 1 second unit
Setbacks (minimum)		
Front ²	15 ft. to living space or side wall of garage; 12.5 ft. to porch 18 ft. min. driveway depth with roll-up garage	10 ft. to first-floor living space or side wall of garage 7.5 ft. to porch, but in no case may encroach into a PUE 15 ft. to second-floor living space 18 ft. min. driveway depth with roll up garage
Sides ² Interior Lots: Corner Lots:	5 ft. 5 ft. interior side 12.5 ft. street side on first floor 15 ft. street side on second floor	5 ft. 5 ft. interior side 10 ft. street side on first floor 13 ft. street side on second floor
Rear	10 ft. minimum with minimum usable open space of 700 s.f. or 500 s.f. where a usable front porch is provided ³	10 ft. minimum with minimum usable open space of 500 s.f. where a usable front porch is provided ³
Coverage (maximum)		
Site Coverage	None ³	None ³
Height (maximum)		
Height	35 ft.	35 ft.
Supplemental Design Standards		
1. Front Yard Stagger	None required, but optional per unit design	
2. Stagger for 3 rd Car Garages	2 ft. between 3 rd car bay and two-car garage	
3. Two-story unit mix	No limit	
4. Separation Between Second Story Elements	A minimum of 10 feet shall be provided between second story elements of adjacent two-story dwellings	
5. Building Exterior	Architectural treatment shall be applied to all elevations of a building. At a minimum, all doors, windows and other wall openings shall be trimmed consistent with the architectural style, consistent with the Compact Residential Design Guidelines provided in the City's adopted Community Design Guidelines. Panelized windows or other architectural treatment shall be used on all garage doors.	

- 1 Sidewalk separated from back of curb by 6-foot planter strip
- 2 Front setback (and side setback where adjacent to street) is measured from back of walk. Fence side yard setback is 5' from back of walk where facing a street. In the absence of a sidewalk, setback is measured from edge of right-of-way.
- 3 The rear and side yards may be utilized to meet the minimum usable open space provided the minimum dimension, measured perpendicular to the applicable rear or side yard is ten (10) feet. Maximum coverage is a function of lot size, required setbacks and usable open space. A minimum usable open space of 500 s.f. may be applied where a front porch is provided with minimum dimension of 6 ft. x 10 ft. exclusive of entry way.
4. Variations to the standards and other housing product types may be permitted through approval of a Design Review Permit for Residential Subdivisions (DRRS).

Figure 10-5a: Village Center MDR (RS/DS) Single Family Detached Plan Details

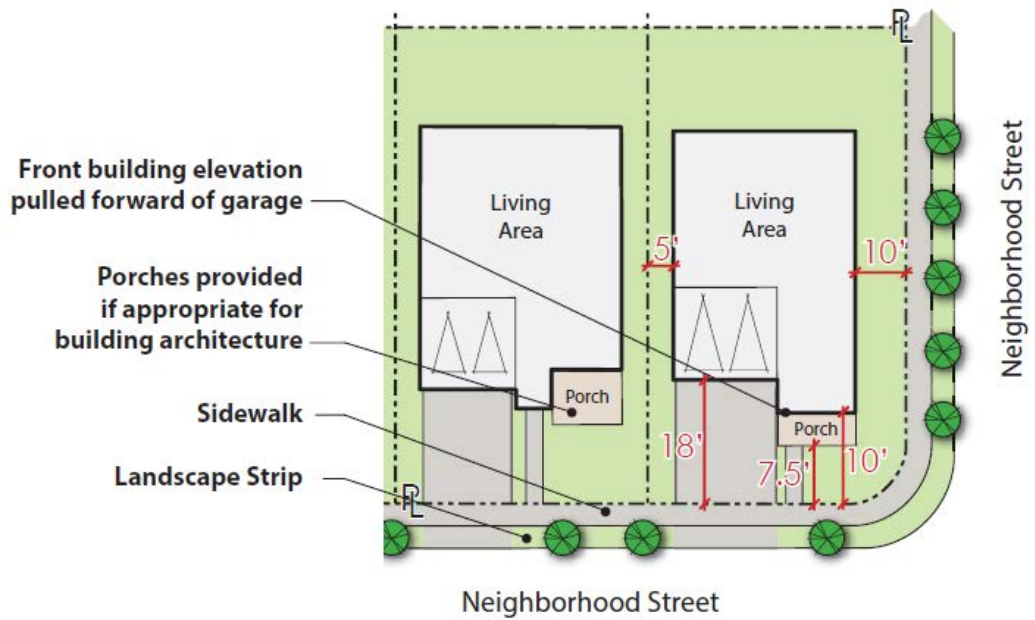


Figure 10-5b: Village Center MDR (RS/DS) Single Family Side Elevation Details

Additional design requirements for side elevations:

All exterior building elevations that face a public street, including side elevations, shall incorporate enhanced architectural design features, which may include building articulation, recesses, or other stylistic elements that enrich the visual design of the facade. Examples of homes with enhanced side elevations are provided below:



Figure 10-6: Front Driveway Prohibited Access Locations

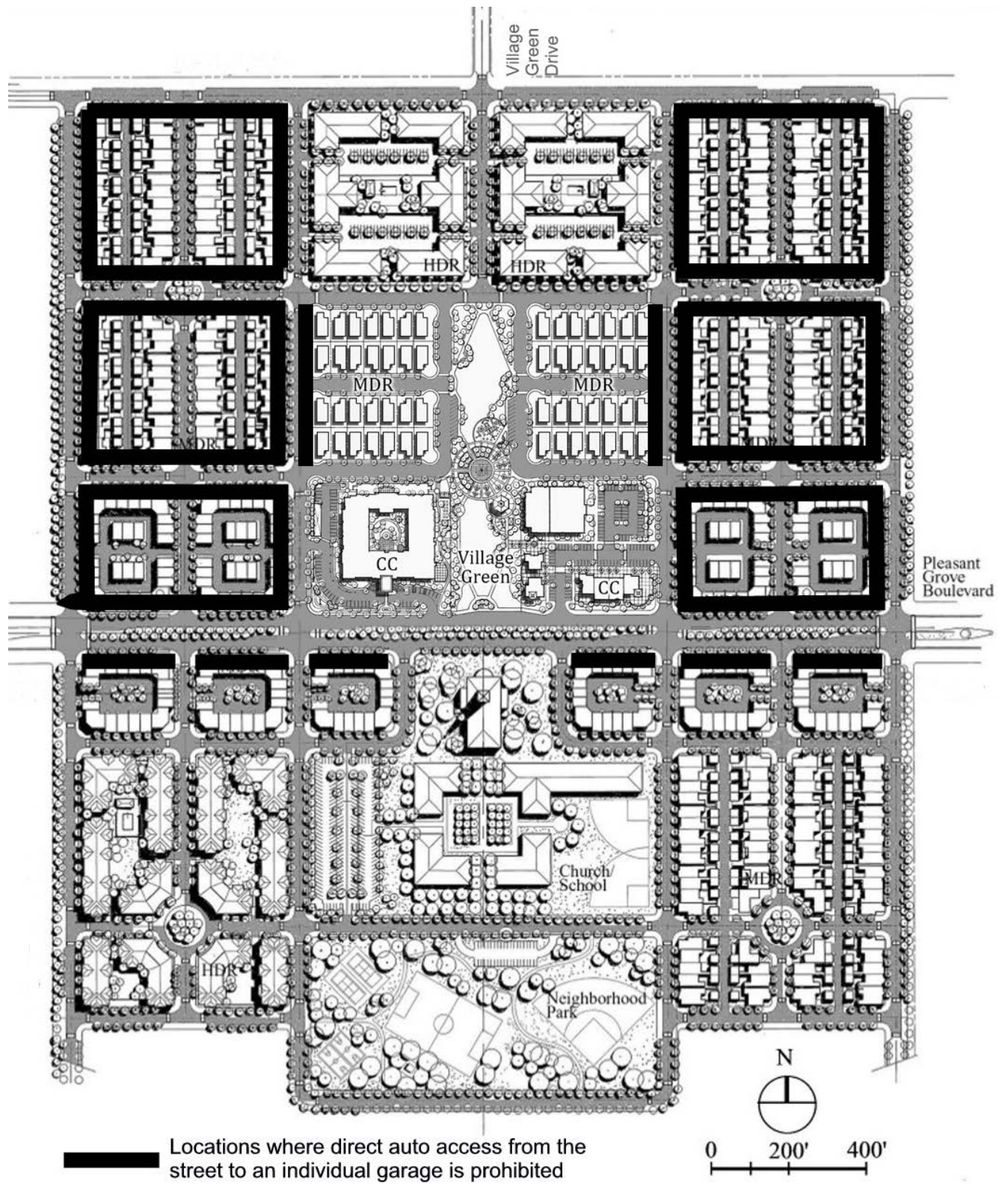


Figure 10-7: Village Center MDR (R3/DS) Front Setback-Cross Sections

Front setback min. of 10' & maximum of 15' from front P/L

Open porches may project into the front yard setback a maximum of 6'

Maximum height of the porch not to exceed 4'-6" above the adjacent curb



Front setback min. of 10' & maximum of 15' from front P/L

Open porches may project into the front yard setback a maximum of 6'

Maximum height of the porch not to exceed 4'-6" above the adjacent curb



High Density Residential (VC-HDR)	
Density Range:	13.0 dwelling units per acre and higher
Applied Zoning District:	R3/DS: Attached Housing/Design Standards Overlay
Description:	<p>High Density Residential (HDR) land use is intended to accommodate attached multifamily housing. Similar to MDR, HDR units are required to have a strong orientation toward the street. A variety of higher density housing types are appropriate if designed with front forward architecture which includes front entry doors and porches facing the street, and off-street parking located in the rear of the lot and accessed from alleys or internal driveways. Typical unit types may include apartments, townhomes and condominiums. Figure 10-8 includes pictures of potential HDR unit types.</p> <p>Table 10-2 includes specific unit allocations and resulting densities for each HDR parcel. While it is anticipated that unit types may be mixed on a large lot parcel, the overall density shall average that prescribed on Table 10-2. In no case may a specific unit type on any HDR large lot parcel be below the density range specified by the HDR land use (13.0 du/ac).</p>
Permitted Uses:	As specified in the City of Roseville Zoning Ordinance.
Development Standards:	The Design Standards Overlay (DS) has been applied to all HDR parcels to provide for the unique nature and variety of housing types anticipated in the Village Center. Development standards are included on Table 10-5. All HDR development is subject to City approval of a Design Review Permit.



Typical HDR Product

Table 10-5: Village Center HDR Development Standards

R3/DS Development Standards (HDR)	
General Characteristics	
Product Type	Attached units (Apartment/Townhome/Condominium)
Garage Access	Via rear alley or auto court
Lot Size	
Area, Interior Lot	Varies
Area, Corner Lot	Varies
Width, Interior	Varies
Width, Corner	Varies
Permitted Density (maximum per lot)	
Residential Density	Per General Plan Land Use Designation
Setbacks and Treatments	
Street Setback ¹ (Figure 10-9)	10 ft. minimum 15 ft. maximum
Off-Street Parking Setback	All parking areas and garages shall be located and accessed internally via alleys or driveways.
Front Porches/ Stoops/Bay Windows (Figure 10-9)	Open porches may project up to 6 feet into front setback; Bay windows or other projections may project up to 3 feet into front setback; Steps/stoop may project to back of sidewalk. A projecting porch/stoop may be elevated but shall not exceed 4 feet 6-inches above the grade of the adjacent sidewalk.
Front Articulation	The residential design intent is to achieve a design that is varied, provides visual interest, and maintains a pedestrian scale for attached and detached housing. The front street façade shall be well articulated and shall reflect the following: <ul style="list-style-type: none"> ▪ <i>Wall Planes</i> – incorporating building projections and recesses, bay windows, front porches, entries, balconies, chimneys, and other elements. Multiple story (two or three story) walls in the same plane should be minimized and in no case shall any multiple-story wall comprise more than 50% of the front façade. ▪ <i>Rooflines</i> – utilizing a variety of roof forms, heights and styles. ▪ <i>Architectural Enhancements</i> – including decorative trim, shutters, columns and other articulation. ▪ <i>Finish</i> – combining multiple materials and textures to enhance architectural detail ▪ <i>Unit Variation</i> – using the above and other elements to create distinction between individual units.
Coverage	
Site Coverage	n/a
Height	
Height (maximum)	45 ft.
Parking (minimum)	
Spaces	Studio Unit - 1 off-street space per unit. One (1) Bedroom Unit- 1 off-street space per unit. Two (2) and above Bedroom Unit - 2 off-street spaces per unit. Age-Restricted Unit - 1 off-street space per unit. Guest parking shall be provided on-street (no additional off-street parking required).
Development Approval	
Processing	Each large-lot parcel shall be planned, processed and approved comprehensively. Large-lot parcels may not be split for purposes of development entitlement processing.

1. Setback adjacent to public roadway measured from back of sidewalk. Units adjacent to public roadways shall front to such roadways.

Note: The City may consider other product types and/or deviations to development standards provided they are consistent with the intent of the Village Center development standards and WRSP Design Guidelines, and provided the products demonstrate superior siting characteristics and architectural design merits.

Figure 10-8: Village Center HDR-Pictures

Variety of roof forms & heights within the facade

Variations in wall plane projections

Balcony details

Porch & Front Entries along the street Facade



Variety of roof forms & heights within the facade

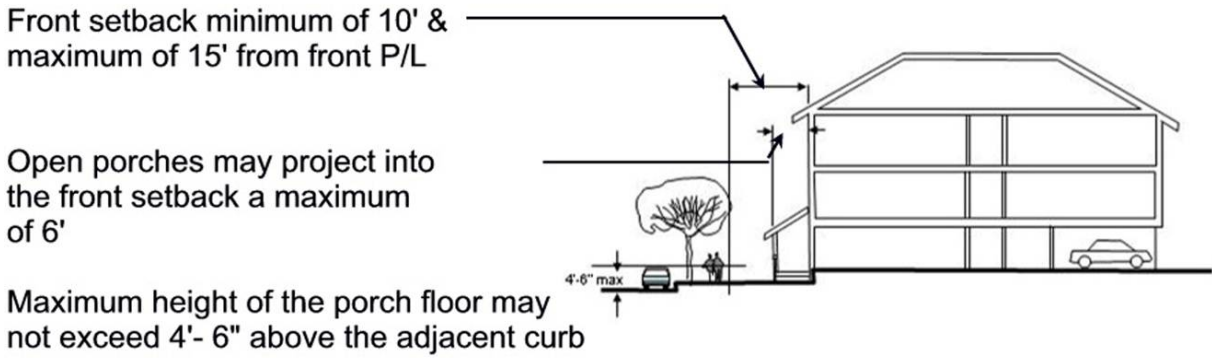
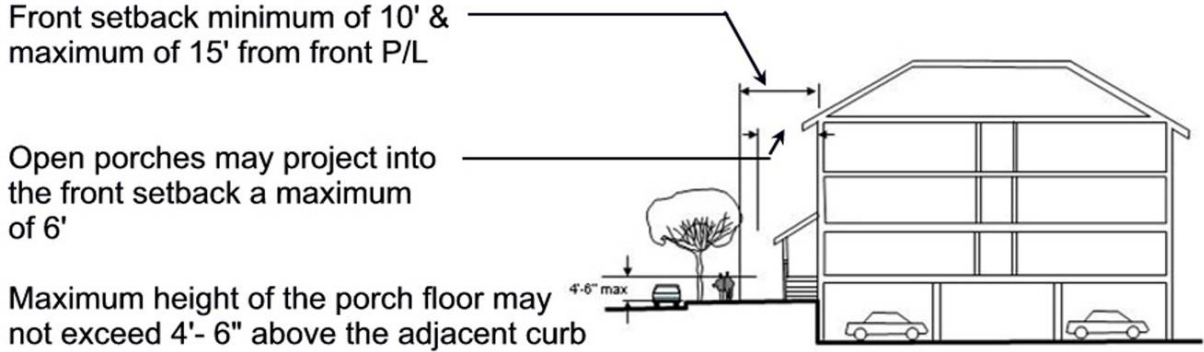
Variations in wall plane projections

Balconies

Porch & Front Entries along the street Facade



Figure 10-9: Village Center HDR Street Setback-Cross Sections



□ RETAIL & SERVICE USES

The integration of commercial land use that can accommodate retail businesses, service uses and community activities is a critical component of the Village Center and a central focus of its design. These uses, in combination with adjacent and mixed residential and public uses, create a diverse community with a variety of options that promote interaction. Both businesses and residents benefit from the proximity of uses, increasing the viability of walking, bicycling and transit as transportation options.

Similar to residential uses, emphasis is placed on the relationship of buildings to the street. Building facades are oriented to the street. Off-street parking is located to minimize visibility from public streets. On-street parking is maximized to provide a visual parking presence while meeting a portion of the parking demand for the retail component of the Village Center. Numerous design elements, such as transparent storefronts, wide sidewalks, traditional style lighting, dense tree canopies, and coordinated street furnishings are provided to create a comfortable and distinctive “main street” setting. Restaurant seating, and other accessory services, are permitted to encroach onto public spaces to enhance an active pedestrian friendly environment and encourage social activity.

Community Commercial (VC-CC)	
Applied Zoning District:	CC/SA-WR: Community Commercial/Special Area Overlay
Description:	The Community Commercial (CC) land use is anticipated to accommodate a variety of retail and service activities. Typical uses may include retail shops, restaurants, local pubs, banks, grocery stores, convenience services, theaters and offices. These uses, within walking distance to and mixed with the Village Center residential, are planned to meet the everyday needs of local residents and promote non-vehicular forms of transportation. In addition, the commercial uses in the Village Center act to draw residents from throughout the community due to their unique form and setting.
Permitted Uses:	The Special Area (SA) overlay has been applied to modify permitted, conditionally permitted and administratively permitted uses. The intent is to provide for a unique mix of uses that support the Village Center concept. This includes permitting residential uses over commercial spaces to create live-work opportunities. In addition, in order to promote retail and other pedestrian attractive service activities along the streetscape, certain uses such as offices require a Conditional Use Permit. Permitted uses are included on Table 10-6.
Development Standards:	Development standards have been modified by the Special Area (SA) overlay to provide for the distinctive nature of the Village Center. Development standards are included on Table 10-7. All commercial development is subject to City approval of an Administrative Permit, provided such development is consistent with the development and design standards contained in Table 10-7.



Table 10-6: CC/SA-WR Permitted Uses

AGRICULTURAL AND OPEN SPACE USE TYPES		CC/SA-WR
Resource Protection & Restoration		-
Resource Related Restoration		-
CIVIC USE TYPES		CC/SA-WR
Community Assembly		-
Community Service		-
Essential Services		P
Hospital Services		
	General Hospital Services	-
	Psychiatric Hospital Services	-
Libraries & Museums, Private		CUP
Public Parking Services		-
Schools		
	College & University	CUP
	Elementary & Secondary	-
	Private Elementary & Secondary	-
Social Services		
	Food Distribution	-
	Food Service	-
	Temporary Resident Shelter	-

RESIDENTIAL USE TYPES		CC/SA-WR
Caretaker/Employee Housing		-
Dwelling		
	Multi-Family	P ⁽²⁾
	Single-Family	-
	Two-Family	-
Family Day Care Home, Small		-
Family Day Care Home, Large		-
Single Room Occupant		-
COMMERCIAL USE TYPES		CC/SA-WR
Adult Business Establishments		-
Animal Sales & Services		
	Grooming & Pet Stores	P
	Kennels	-
	Veterinary Clinics	P
	Veterinary Hospital	-
Automotive & Equipment		
	Automotive Rentals	-
	Automotive Repairs	-
	Automotive Sales	-
	Car Wash and Detailing	-
	Commercial Parking	-
	Heavy Equipment Rental, Repair and Sales	-
	Equipment Repair	-
	Gasoline Sales	-
Banks & Financial Services ⁽³⁾		P
Bars & Drinking Places		P
Broadcasting and Recording Studios		-
Building Materials Stores		-
Business Support Services		P
Commercial Recreation		
	Amusement Center	P
	Indoor Entertainment	P
	Indoor Sports and Recreation	P
	Outdoor Entertainment	-
	Outdoor Sports and Recreation	-
	Large Amusement Complexes	-
Community Care Facility ⁽¹⁾		P
Day Care Center		P
Eating and Drinking Establishment		
	Fast Food with Drive Through	-
	Convenience	P
	Full Service	P
Food & Beverage Retail Sales		P
Funeral & Internment Services		-
Lodging Services		-
Long Term Care Facility		-
Maintenance and Repair		P
Medical Services		
	General	P
	Substance Abuse Treatment Clinic	-

COMMERCIAL USE TYPES (cont.)		CC/SA-WR
Neighborhood Commercial		-
Nightclubs		-
Nursery, Retail		-
Offices, Professional ^{(3) (4)}		CUP
Personal Services ⁽⁵⁾		P
Retail Sales and Services ^{(3) (6)}		P
Specialized Education & Training		
	Specialty Schools	CUP
Storage, Personal Storage Facility		-

INDUSTRIAL USE TYPES		CC/SA-WR
Laundries, Commercial		-
Printing & Publishing		-
Research Services		-
Wholesale & Distribution, Light		-

TRANSPORTATION AND COMMUNICATION USE TYPES		CC/SA-WR
Antennas & Communications Facilities		
	Developed Lot	-
	Undeveloped Lot	-
Heliport		-
Intermodal Facilities		-

All highlighted uses have been modified by the WRSP SA Overlay.

- (P) Principally Permitted
- (CUP) Permitted through approval of a Conditional Use Permit
- (A) Administratively Permitted
- (-) Not Permitted

Notes:

- (1) Permitted only on Parcel W-32.
- (2) Permitted on second floor only.
- (3) Cash advance and bail bonds are not permitted uses within the Banks and Financial Services, Professional Office and Retail Sales and Services use types.
- (4) A maximum of 25 percent of the total square footage developed on Parcel W-33 can include Professional Office Uses on the ground floor.
- (5) Tattoo shops and body piercing shops are not permitted uses within the Personal Services use type.
- (6) Smoke shops, hookah lounges and smoking lounges are not permitted uses within the Retail Sales and Services use type.

Table 10-7a: Modified CC Development & Design Standards for Parcel W-32

CC/SA-WR Development & Design Standards for Parcel W-32	
Sidewalks	
Design Intent: <i>To provide adequate space to accommodate and promote pedestrian related activity.</i>	
Sidewalks	Pleasant Grove Boulevard: 8-feet minimum, attached to back of curb Village Center Drive: 6-feet minimum, detached from curb by 6'-wide planter strip
Building Setbacks	
Design Intent: <i>To ensure that buildings have a strong visual presence along major streets</i>	
Front/ Pleasant Grove Blvd.	20-feet from back of curb minimum
Side & Rear	Adjacent to Street – 12.5-feet from back of sidewalk Adjacent to Village Green Park – 10-feet from property line abutting park
Decorative Walls	5-feet minimum from back of sidewalk
Entries, Building Articulation, & Architectural Features	
Design Intent: <i>To ensure that building entrances are highly-visible along primary street frontages and that building architecture incorporates traditional elements such as a base/bulkhead, body with windows, and roofline/cornice, which create visual interest and high-quality architecture.</i>	
Entries	Primary building entrances facing Pleasant Grove Boulevard or Village Center Drive shall be clearly articulated and discernable elements of the building.
Building Articulation	Street-facing facades (or within public view) shall be well articulated through the use of a variety of roof forms, building projections and recesses, window styles, decorative trim, awnings, cornice and belt courses, paint colors, and/or finish materials; Corner parcels are encouraged to incorporate rounded or cut corners and other architectural features. Added height at corner elements is permitted to create a focal point (see Height Exception Standards below).
Rooflines	Shall be varied and capped with a strong cornice line, roof treatment or combination of treatments.
Side/Rear Facades	Building facades not facing primary street frontages shall be designed to present an inviting façade to adjacent parcels and surrounding streets; Service doors and areas shall be architecturally designed as an integral part of the building.
Lighting	Shall be designed as an integral part of the building façade to provide an ambient level of light on the street. Lighting may provide low-level wash on the face of the building.
Height	
Design Intent: <i>To create building walls and massing that define, but do not overwhelm, public spaces.</i>	
Height Limit	35-feet maximum;
Exceptions	Towers, cupolas, steeples, dormers, spires, flagpoles, and similar architectural elements if the element is integrated into the overall architectural design and occurs at significant locations, such as at primary building entrances or building corners.
Parking	
Design Intent: <i>To provide adequate parking while minimizing the visual impacts of off-street parking areas.</i>	
Off-Street Parking	Per requirements of City of Roseville Zoning Ordinance.
Screening	The perimeter of off-street parking areas abutting a street shall have a minimum 6-foot wide landscape setback consisting of any combination of a low concrete/masonry wall, landscaped berm, and/or hedge that is 4-feet high.

Figure 10-10: Concept Plan for Commercial Parcel W-32

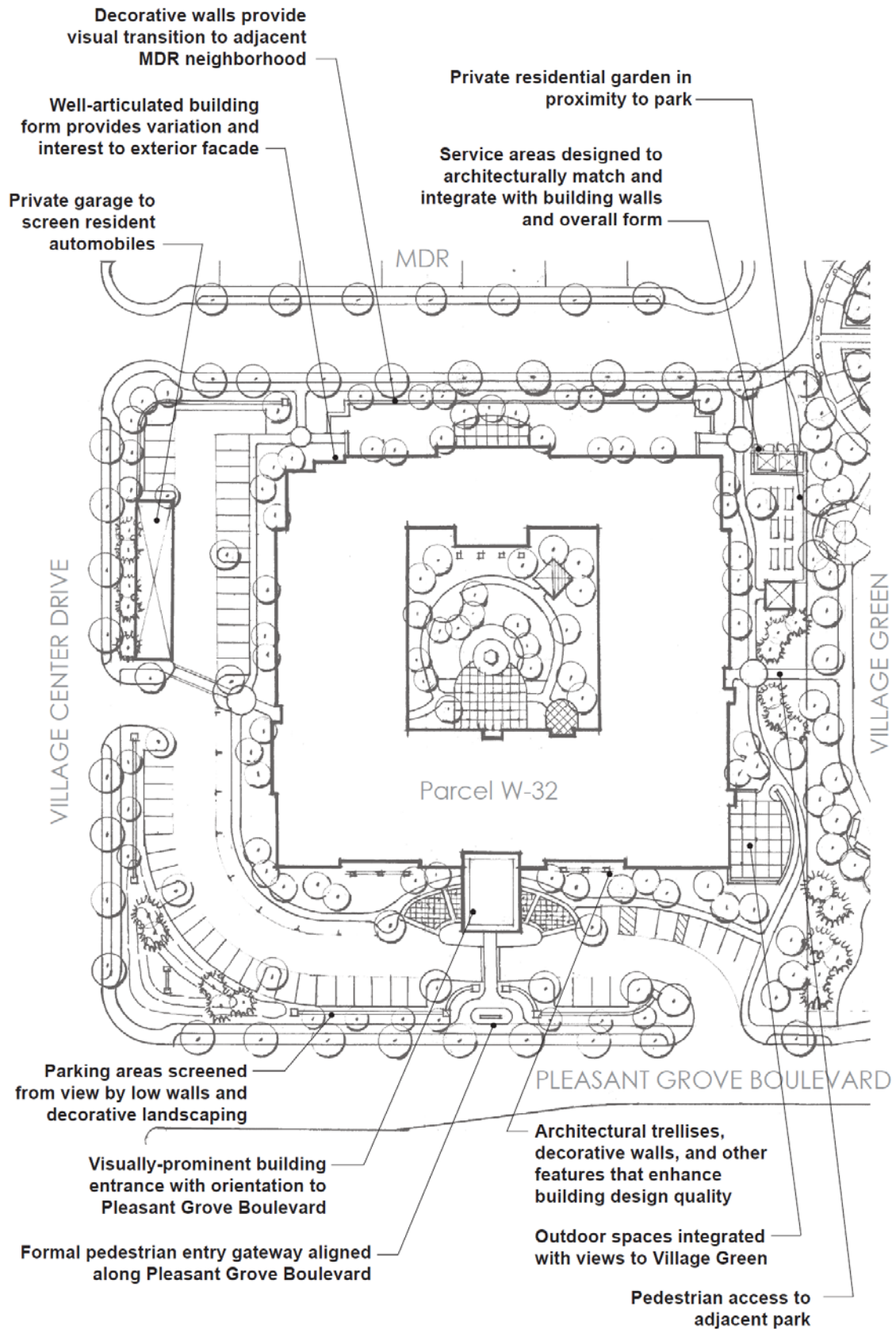


Figure 10-11: Design Elements for Commercial Buildings on Parcel W-32

Articulated massing creates smaller building forms that add architectural diversity

Varied roof lines respond to different building forms

Use of color and trim elements enhance visual appearance

Building entrance defined with prominent architectural feature

Design incorporates distinct base and body elements



Windows incorporate shutters or other design elements to highlight their appearance

Decorative trellises enhance appearance of building facade

Outdoor patio areas provide semi-private spaces for seating

Low, decorative walls visually define public realm from private spaces



Table 10-7b: Modified CC Development & Design Standards for Parcel W-33

CC/SA-WR Development & Design Standards for Parcel W-33	
Sidewalks	
<i>Design Intent: To provide adequate space to accommodate and promote pedestrian related activity.</i>	
Sidewalks	Pleasant Grove Boulevard: 8-feet minimum, attached to back of curb Village Plaza Drive.: 6-feet minimum, detached from curb by 6- foot-wide planter strip
Building Setbacks	
<i>Design Intent: To ensure that buildings and associated activities are pulled forward toward the sidewalk and street to create a defined street edge.</i>	
Front/ Pleasant Grove Setback	20-feet from back of curb (8 foot attached walk and 12 foot landscaped planter).
Front Parking Setback	Minimum 6-foot landscape setback from back of sidewalk with screening. See Parking Screening Standards below.
Side & Rear Setbacks	Adjacent to Street – 12.5-feet from back of sidewalk Adjacent to Park – 10-feet
Entries, Building Articulation, & Architectural Features	
<i>Design Intent: To ensure that entrances are well-defined elements of building design, that activity spaces are visually prominent and relate to building function, and that building architecture incorporates traditional elements such as a base/bulkhead, body with windows, and roofline/cornice, which create visual interest and high-quality architecture.</i>	
Entries	Entries shall be clearly articulated and discernable elements of the building design. Porticos are encouraged to help define retail entries, where appropriate for the architectural style.
Building Articulation	Exterior building design shall be well articulated through the use of a variety of roof forms, building projections and recesses, window styles, decorative trim, awnings, cornice and belt courses, paint colors, and/or finish materials; Corner parcels are encouraged to incorporate rounded or cut corners, towers, or other architectural features that enhance the architectural quality of the building. Added height at corner elements is permitted to create a focal point (see Height Exception Standards below).
Rooflines	Shall be varied and capped with a strong cornice line, roof treatment or combination of treatments.
Awnings	Are encouraged as an element of building architecture and shall maintain a minimum clearance of 9-feet above the sidewalk.
Signage	Shall complement the style of architecture and be integrated into the building's design; Acceptable sign types include, halo, lit or reverse pan channel, and indirectly lit panel signs; Internally illuminated cabinet signs, pan channel signs and exposed railways are prohibited; Blade or shingle-type signs are encouraged under canopies and awnings as close as practical to the horizontal middle of the building;
Side/Rear Facades	Building facades not facing primary street frontages shall be designed to present an inviting façade to adjacent parcels and surrounding streets; Service doors and areas shall be designed as an integral part of the architectural building façade.

CC/SA-WR Development & Design Standards for Parcel W-33	
Lighting	Shall be designed as an integral part of the building façade to provide an ambient level of light. Lighting may provide low-level wash on the face of the building.
Service Entries	Service or delivery entries should be discouraged along the interface with the park. If located along this edge, these entries shall be architecturally integrated into the building design such that they do not appear as “back” doors.
Height	
Design Intent: <i>To create building walls and massing that define, but do not overwhelm, the public spaces.</i>	
Height Limit	35-feet maximum; In no case shall the top of a building parapet or roof façade along the storefront be less than 16-feet.
Exceptions	Towers, cupolas, steeples, dormers, spires, flagpoles, and similar architectural elements if the element is integrated into the overall architectural design and occurs at significant locations, such as at primary building entrances or at building corners.
Pedestrian Ways/Plazas	
Design Intent: <i>To link the individual commercial buildings to the off-street parking areas and to provide a clearly-defined pedestrian connection from the retail center to the Village Green.</i>	
Location	An internal pedestrian way is encouraged between the off-street parking areas, Village Plaza Drive, and the Village Green. The pedestrian ways should be clearly defined through the use of sidewalks, planters, or other site features that are visually prominent in the retail center. Outdoor plazas or seating areas are encouraged as part of site design to enhance building architecture and visually activate building edges and park interface.
Width	Pedestrian ways shall have a minimum width of 6-feet and a maximum width of 25-feet.
Treatment	Paving, landscaping and illumination along pedestrian ways shall be designed with site landscape architecture; Porticos or trellises are encouraged to help define pedestrian ways.
Parking	
Design Intent: <i>To provide adequate parking while minimizing the visual impacts of off-street parking areas.</i>	
Off-Street Parking	Per requirements of City of Roseville Zoning Ordinance.
Screening	The perimeter of off-street parking areas abutting a street shall have a minimum 6-foot wide landscape setback consisting of any combination of a low concrete/masonry wall, landscaped berm, and/or hedge that is 4-feet high.

Figure 10-12: Concept Plan for Commercial Parcel W-33

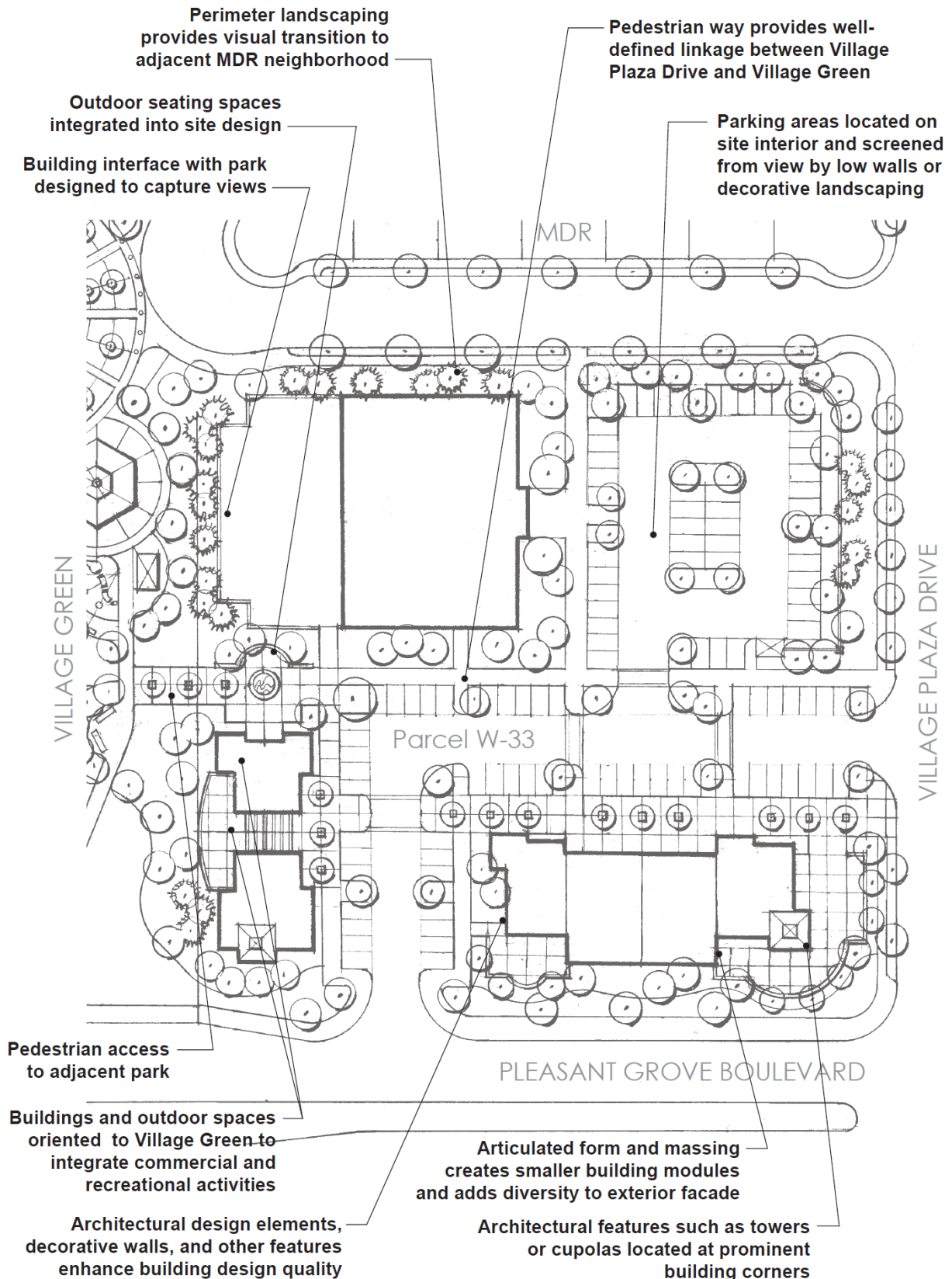


Figure 10-13: Design Elements for Commercial Buildings on Parcel W-33

- Varied roof lines respond to different building forms
- Articulated massing creates smaller building forms that add architectural diversity
- Building entrance defined with prominent architectural feature



- Design incorporates distinct base, body, and cap facade elements
- Decorative awnings, lighting and other features enhance appearance of building facade
- Variation in building colors reinforce architectural style

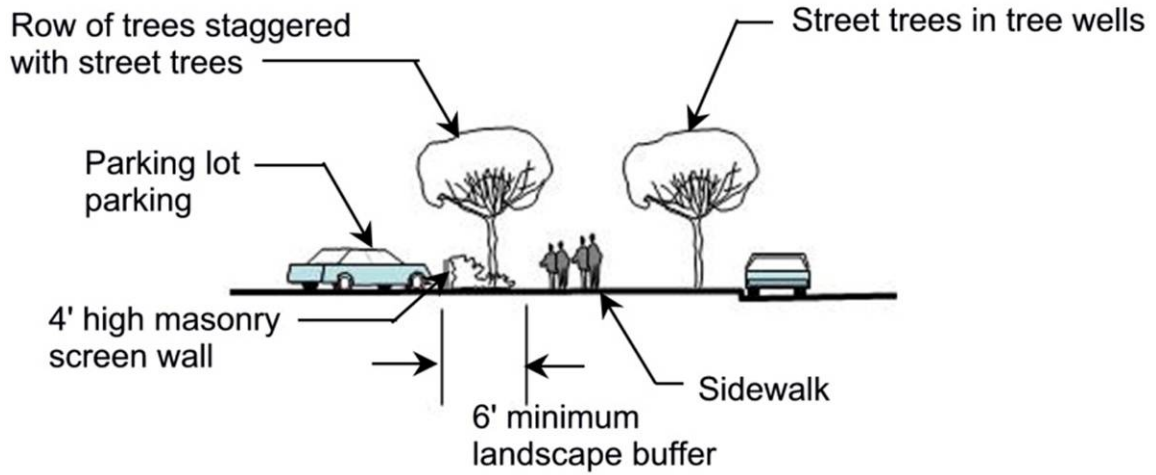


- Building form and massing incorporates towers or other articulated elements that strengthen architectural style
- Outdoor patio areas provide semi-private spaces for seating
- Low, decorative walls visually define public realm from private spaces



{Figures 10-14 and 10-15 removed September 6, 2017 by Resolution 17-393}

Figure 10-16: Village Center Parking Screening-Cross Section



□ PUBLIC & QUASI-PUBLIC SPACES



The Village Center provides public and quasi/public spaces for people to gather and to reinforce community identity. The centrally located Village Green, a 3.71-acre traditional urban park element, provides the visual focus of the Village Center. The Village Green is intended to accommodate passive recreation, provide a visual respite for residents, shoppers and employees, and form a community gathering place. It includes a central pavilion as a venue for a wide variety of community activities,

including concerts, fairs, exhibits, markets and other events that will bring the community to the Village Center. The form of the Village Center calls for the adjacent commercial uses to face onto the Village Green. A second park site, a City neighborhood park, is included on the south end of the Village Center. This 8.98 acre park is intended to meet the active recreational needs of residents within the Village Center and adjacent neighborhoods.

The Village Center incorporates a 10.83-acre Public/Quasi-Public site to be used for a church and private school. The church and private school create another identifying community element and activity center. The church site has been positioned to face the Village Green from across Pleasant Grove Boulevard, and is aligned with the axis of Village Green Drive creating a visual focal point. The Church is required to include a spire, tower or other architectural element to align with the visual centerline of Village Green Drive and provide a focal point for the Village Center.



Park & Recreation (VC-P/R)	
Applied Zoning District:	PR – Parks & Recreation
Description:	The Village Center includes two park sites, the Village Green and a traditional neighborhood park. These facilities are further described in Section 10.6. While located within the Village Center, these parks are components of the overall WRSP park and recreation program discussed in the Public Services Plan, Section 8.
Permitted Uses:	As specified in the City of Roseville Zoning Ordinance.
Development Standards:	As specified in the City of Roseville Zoning Ordinance.

Public/Quasi-Public (VC-P/QP)	
Applied Zoning District:	P/QP/SA-WR
Description:	The Public/Quasi-Public land use is anticipated to accommodate a church and private school.
Permitted Uses:	The Special Area (SA) overlay has been applied to modify permitted, conditionally permitted and administratively permitted uses. The intent is to focus permitted uses on the church and associated private school. Permitted uses are included on Table 10-8.
Development Standards:	As specified in the City of Roseville Zoning Ordinance. The church is required to include a spire, tower or other architectural element to align with the visual centerline of Village Green Drive to provide a focal element and point of orientation for the Village Center. All P/QP development is subject to City approval of a Design Review permit.

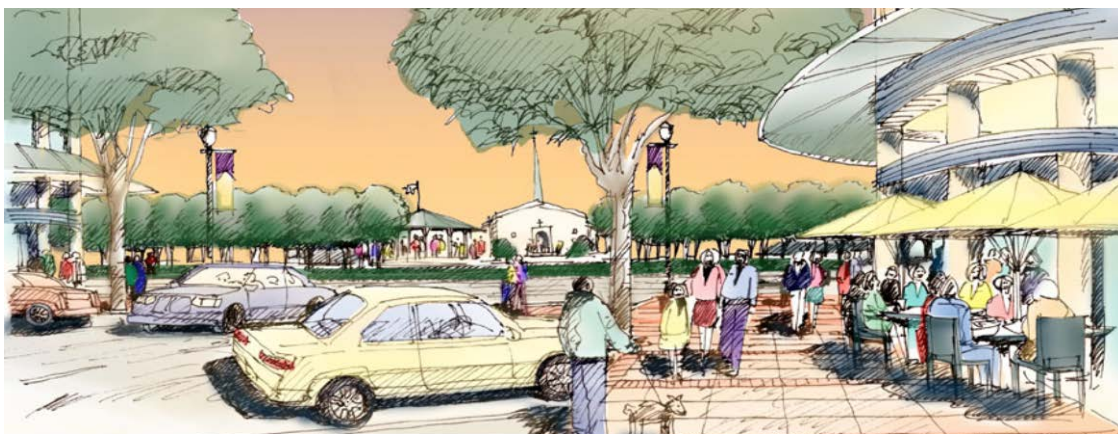


Table 10-8: P/QP/SA-WR Permitted Uses

AGRICULTURAL AND OPEN SPACE USE TYPES		P/QP/SA-WR
Agricultural		-
Animal Keeping		-
Resource Protection & Restoration		-
Resource Related Restoration		-

CIVIC USE TYPES		P/QP/SA-WR
Community Assembly		P
Community Service		P
Essential Services		P
Hospital Services		
General Hospital Services		-
Psychiatric Hospital Services		-
Intensive Public Facilities		-
Libraries & Museums, Private		-
Public Parking Services		P
Schools		
College & University		-
Elementary & Secondary		-
Private Elementary & Secondary		P

RESIDENTIAL USE TYPES		P/QP/SA-WR
Caretaker/Employee Housing		A
Dwelling, Single-Family		A

COMMERCIAL USE TYPES		P/QP/SA-WR
Commercial Recreation		
Indoor Sports and Recreation		P
Outdoor Entertainment		CUP
Outdoor Sports and Recreation		CUP
Large Amusement Complexes		-
Day Care Center		P

All highlighted uses have been modified by the WRSP SA Overlay.

- (P) Principally Permitted
- (CUP) Permitted through approval of a Conditional Use Permit
- (A) Administratively Permitted
- (-) Not Permitted

□ MINOR RESIDENTIAL DENSITY ADJUSTMENTS

Minor Residential Density Adjustments are permitted within the Village Center subject to the requirements of Section 4.3, with the following additions:

1. The Minor Density Adjustment may not result in a reduction of units within the Village Center; and
2. The Minor Density Adjustment may only be approved if determined by the Planning Director that it improves the ability to achieve the Planning Principles for the Village Center and produce the residential product types envisioned.

10.4 AFFORDABLE HOUSING PLAN

Consistent with the General Plan affordable housing goal, ten percent (10%) of the units in the WRSP have been designated for middle-, low- and very-low income households. The WRSP affordable housing goal has been allocated to specific residential parcels. This includes the allocation of 75 low-income rental and 75 very low-income rental units to Parcel W-25 in the Village Center. Affordable units shall be provided in accordance with Section 5 of the Specific Plan. Included is the requirement for a *Supplemental Affordable Housing Development Agreement* to detail specific requirements and obligations prior to the issuance of any building permits for Parcel W-25.

10.5 CIRCULATION PLAN

The Village Center is a district where preference is given to the pedestrian rather than the automobile. The mix, proximity, and intensity of uses, along with the underlying grid street pattern, encourage alternative forms of transportation including walking, bicycling and transit. All uses are in easy walking distance, within 1,500 feet of the Village Green and a central transit stop. Numerous design elements, including wide sidewalks, enhanced pedestrian crossings, dense tree canopies, street furnishings, pedestrian scale lighting, signage and front forward building designs are intended to promote a comfortable and inviting pedestrian environment.

In accordance with the City's General Plan, the Village Center is designated as a Pedestrian District. The intent of the Pedestrian District is to place a greater emphasis on the pedestrian rather than the automobile by implementing measures to improve walkability through enhanced safety, security and convenience within and through the District. The Village Center incorporates numerous elements, from its basic form to unique pedestrian and vehicular circulation standards, which promote walkability and support the intent of the Pedestrian District. In designating the Village Center as a Pedestrian District, the City acknowledges that certain design features may slow the speed of vehicle travel and may reduce the vehicle level of service. The Pedestrian District overlay has been applied to the entire Village Center area.

□ ROADWAYS

The Village Center incorporates a traditional grid circulation pattern providing multiple routes and options for both pedestrians and automobiles. The conventional suburban emphasis on a hierarchy or functional classification of roadways is replaced by a general uniformity of street options. All roadways place equal emphasis on vehicular and pedestrian transportation. The unique nature of the Village Center roadway system necessitates variations to typical City street standards including on-street diagonal parking, wider sidewalks abutting commercial frontages, and sidewalk “bulb-outs” at intersections.

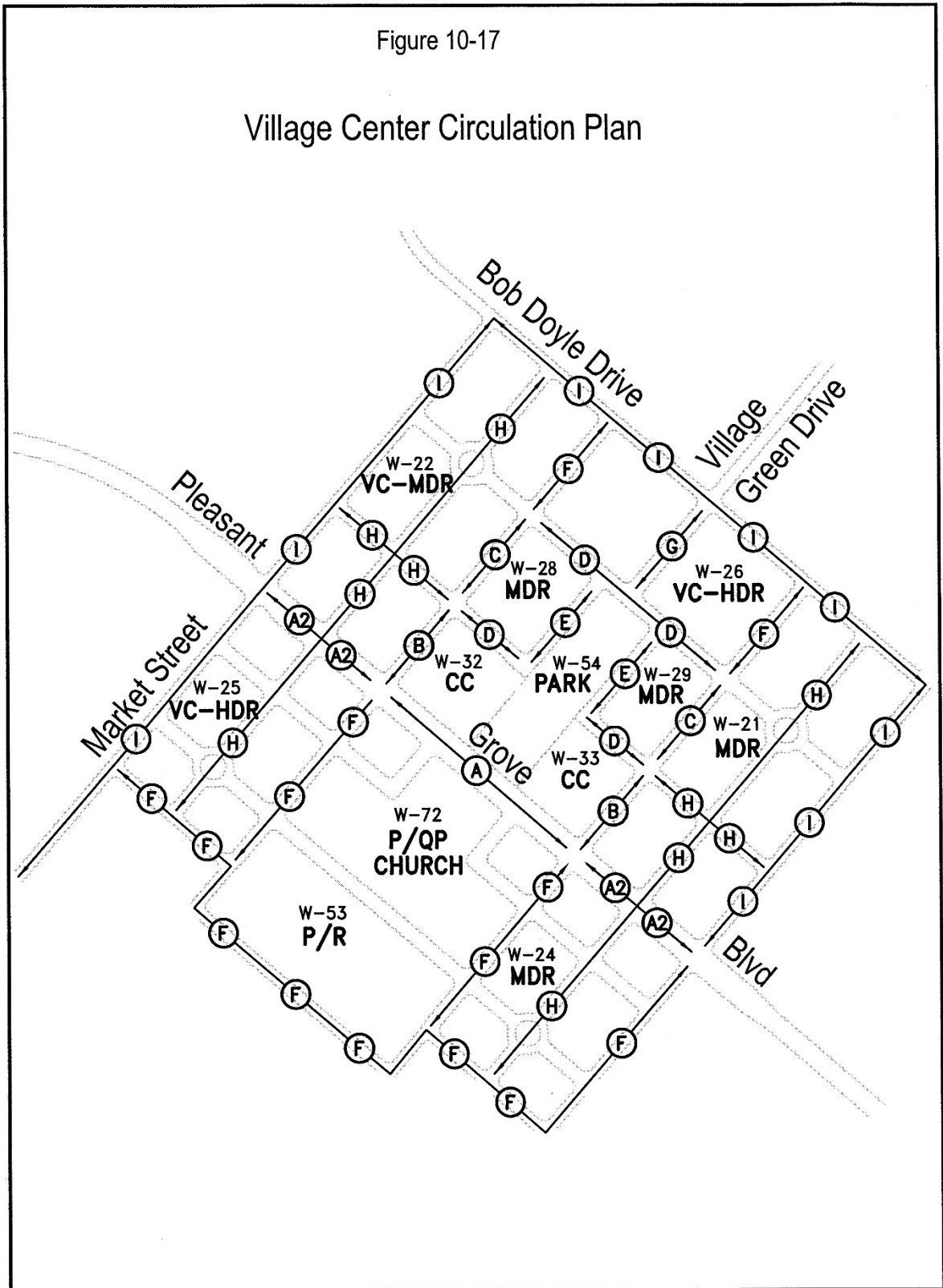
The Village Center’s grid roadway system shall include:

- Short blocks of 300-feet in length or less;
- Straight streets; and
- Intersections at regular intervals.

Roads will be in compliance with emergency vehicle access requirements. Internal streets included within projects on Village Center large lot parcels are required to complement the overall grid network. In addition, coordination is required to ensure that roadways and pedestrian access points in adjacent development connect to Village Center roadways and sidewalks.

The Village Center Circulation Plan is reflected on Figure 10-17, with lane capacity, right-of-way, sidewalk widths, parking and general landscape treatments summarized on Table 10-9. All roadways within the Village Center are required to be in conformance with Figure 10-17 and Table 10-9. Specific roadway sections for Village Center roadways are included as Figures 10-18 through 10-22. Landscape treatments, street light details and other streetscape elements are included in the WRSP Design Guidelines (Section 12).

Figure 10-17: Village Center Circulation Plan



Note: Underlying parcel basemap used in this figure are not regularly updated.

Refer to Table 4-1 for the most up to date unit and acre information.

Table 10-9: Village Center Roadway Summary

Street	Lanes	Right-of-Way	Sidewalk Width	On-Street Parking	Landscape Treatment	Landscape Median	Figure
ARTERIAL ROADWAY							
Pleasant Grove Blvd. (A) (Adjacent to Commercial & Park)	4	92'	6'-8'1	None	6' Planter Strip	30'	10-18
Pleasant Grove Blvd. (A2) (Residential on both sides)	4	92'	6'	None	6' Planter Strip	30'	10-18
COMMERCIAL ROADWAYS							
North/South at Commercial (B)	2	60'	5'-6'	Parallel	5-6' Planter Strip	-	10-20
East/West (D)	2	61'	5'-6'	Parallel	6' Planter Strip	-	10-20
Collector at Perimeter (I)	2	42'	5'	Parallel	5' Planter Strip	-	10-22
RESIDENTIAL ROADWAYS							
North/South Residential (C)	2	59'	5'-6'	Parallel	5-6' Planter Strip	-	10-21
Residential at Park (E)	2	49'	5'	Parallel	6' Planter Strip	-	10-21
Major Residential Street (F)	2	58'	5'	Parallel	5' Planter Strip	-	10-21
Residential Entry Street (G)	2	62'	6'	Parallel	6' Planter Strip	-	10-21
Minor Residential Street (H)	2	54'	5'	Parallel	5' Planter Strip	-	10-22
Alley (public)	1	20'	-	-	-	-	-

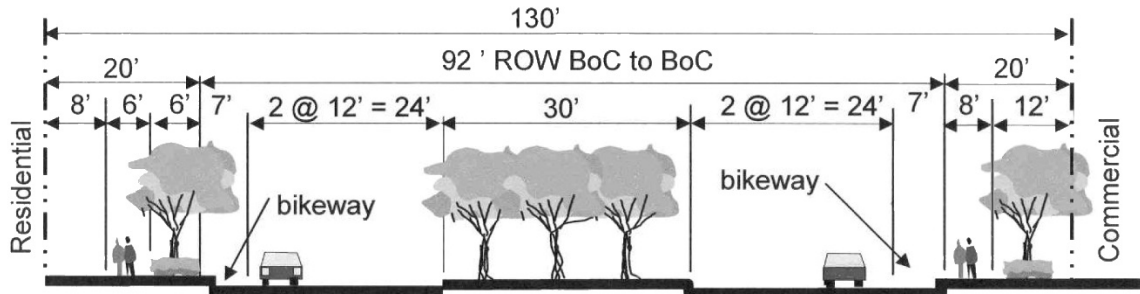
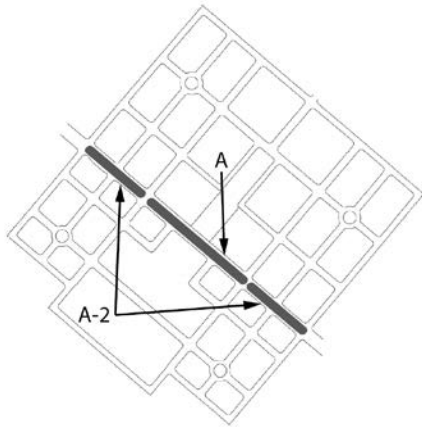
1. 8-foot sidewalk required adjacent to commercial and park; 6-foot sidewalk required adjacent to public/quasi-public.

Note: See WRSP Design Guidelines, for landscape requirements and other details.

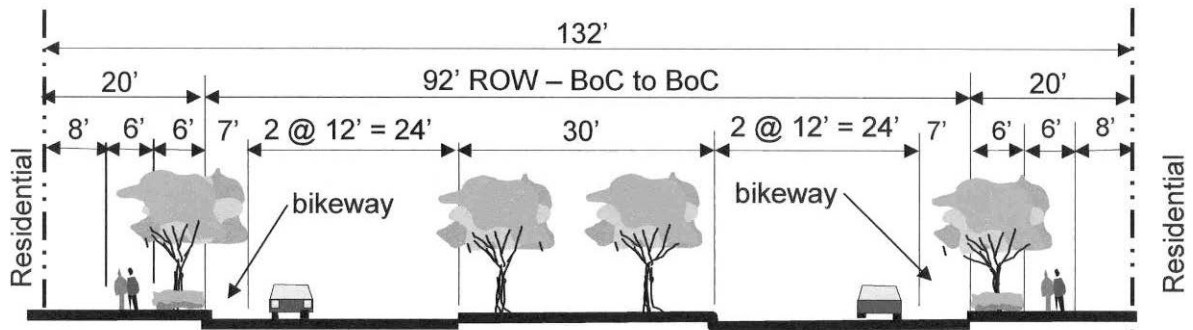


Typical Commercial Roadway

Figure 10-18: Village Center Pleasant Grove Boulevard Street Sections

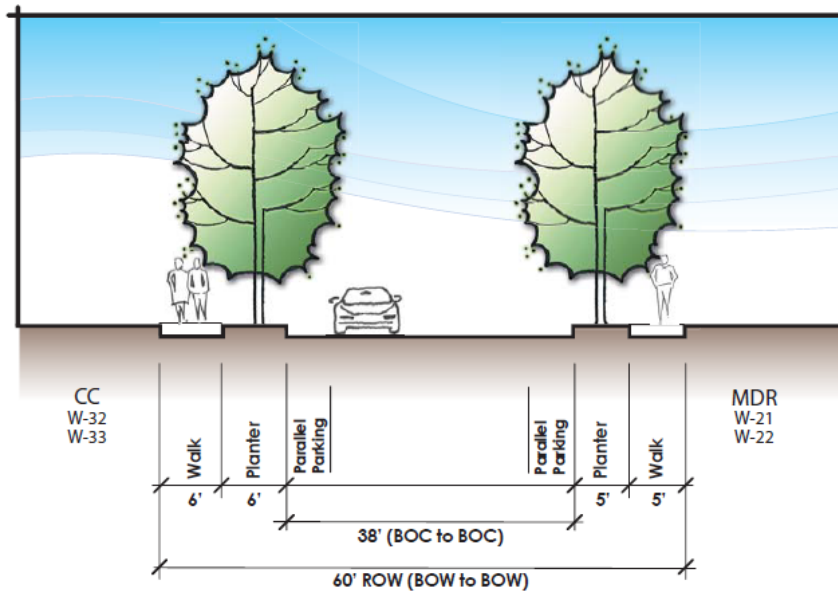
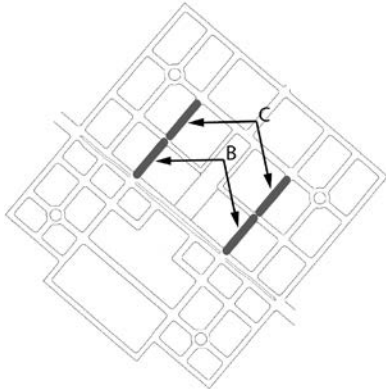


Section A
Pleasant Grove Blvd. @ Commercial

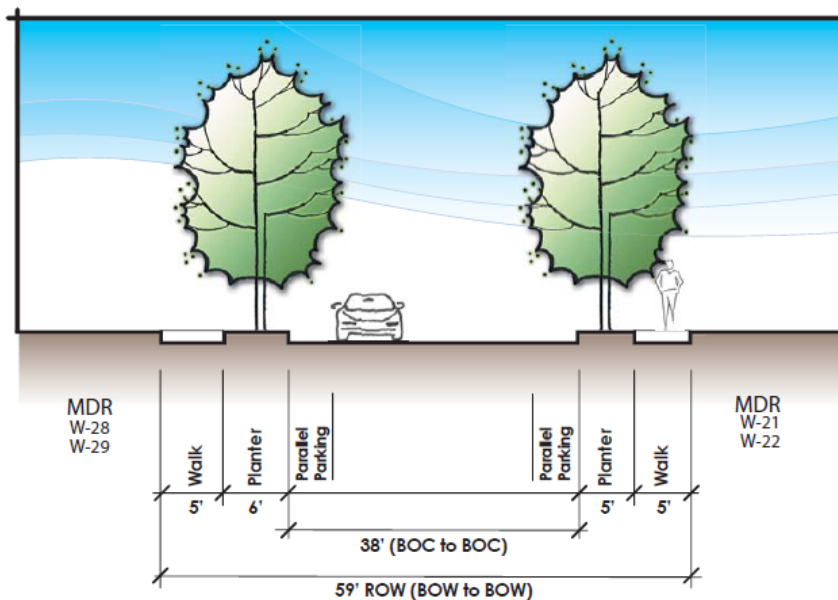


Section A-2
Pleasant Grove Blvd. @ Residential

Figure 10-19: Village Center Drive & Village Plaza Drive Street Sections

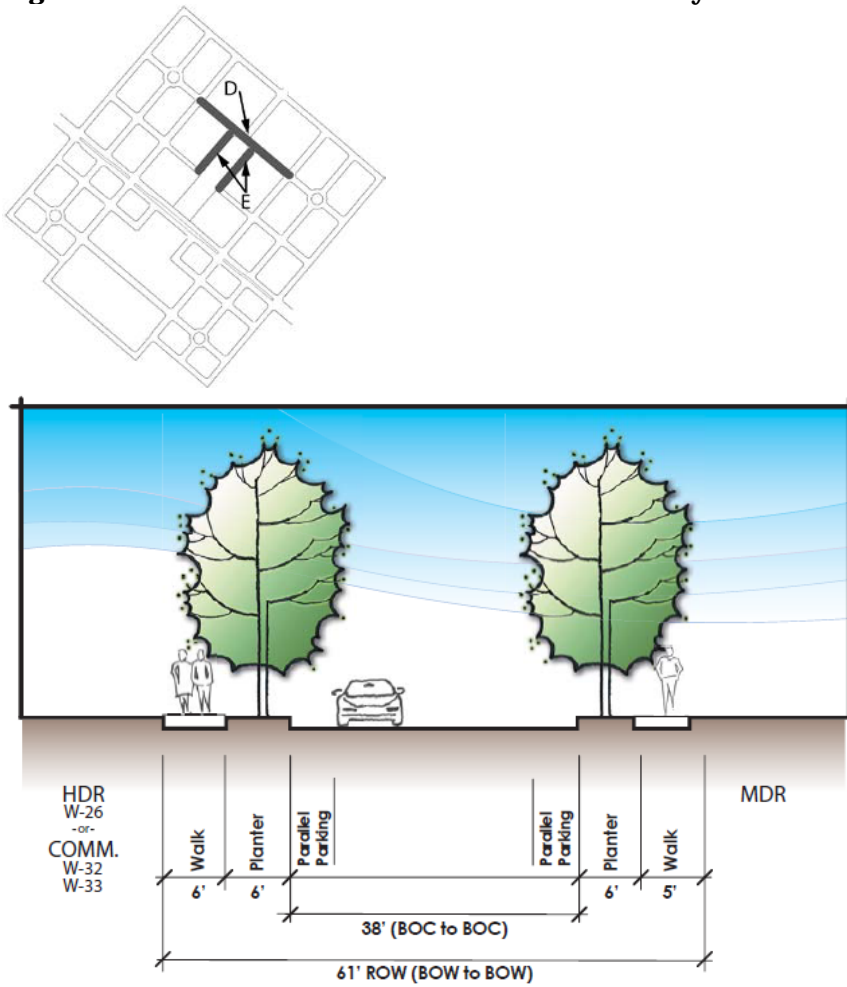


Section B: Village Center Drive and Village Plaza Drive at Commercial/MDR

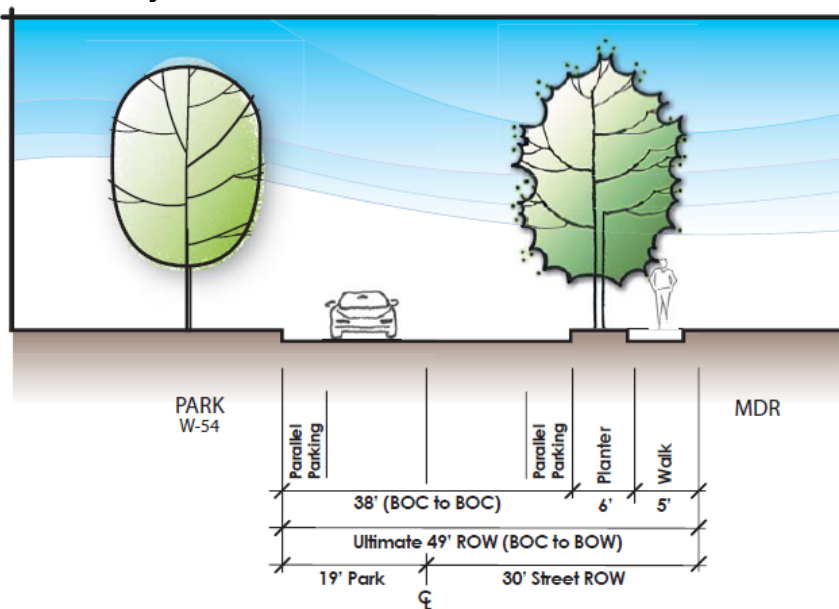


Section C: Village Center Drive and Village Plaza Drive at Residential

Figure 10-20: Park Interface Street Sections at Mayhill Drive & MDR

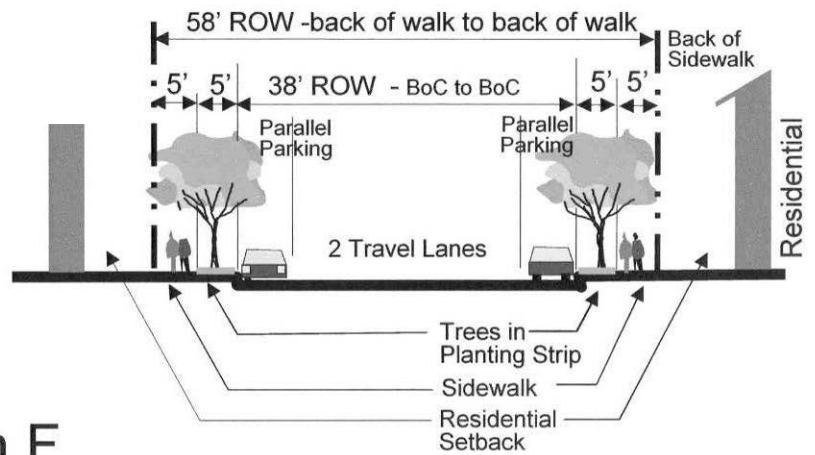
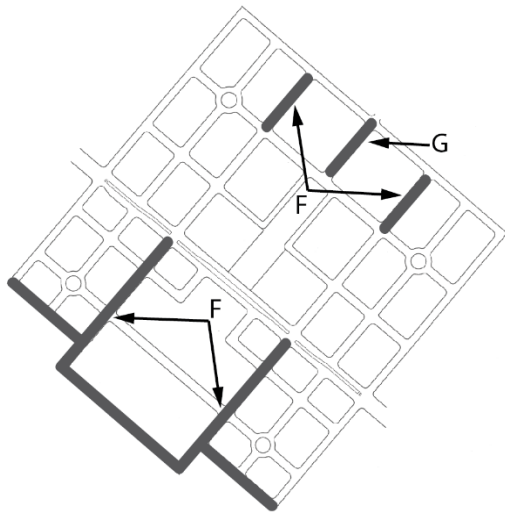


Section D: Mayhill Drive

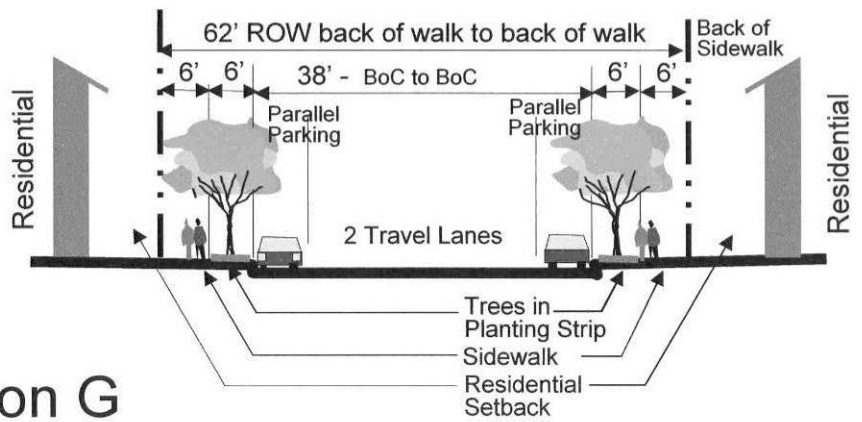


Section E: Village Green and Residential Interface

Figure 10-21: Village Center Master Residential and Entry Residential Street Sections

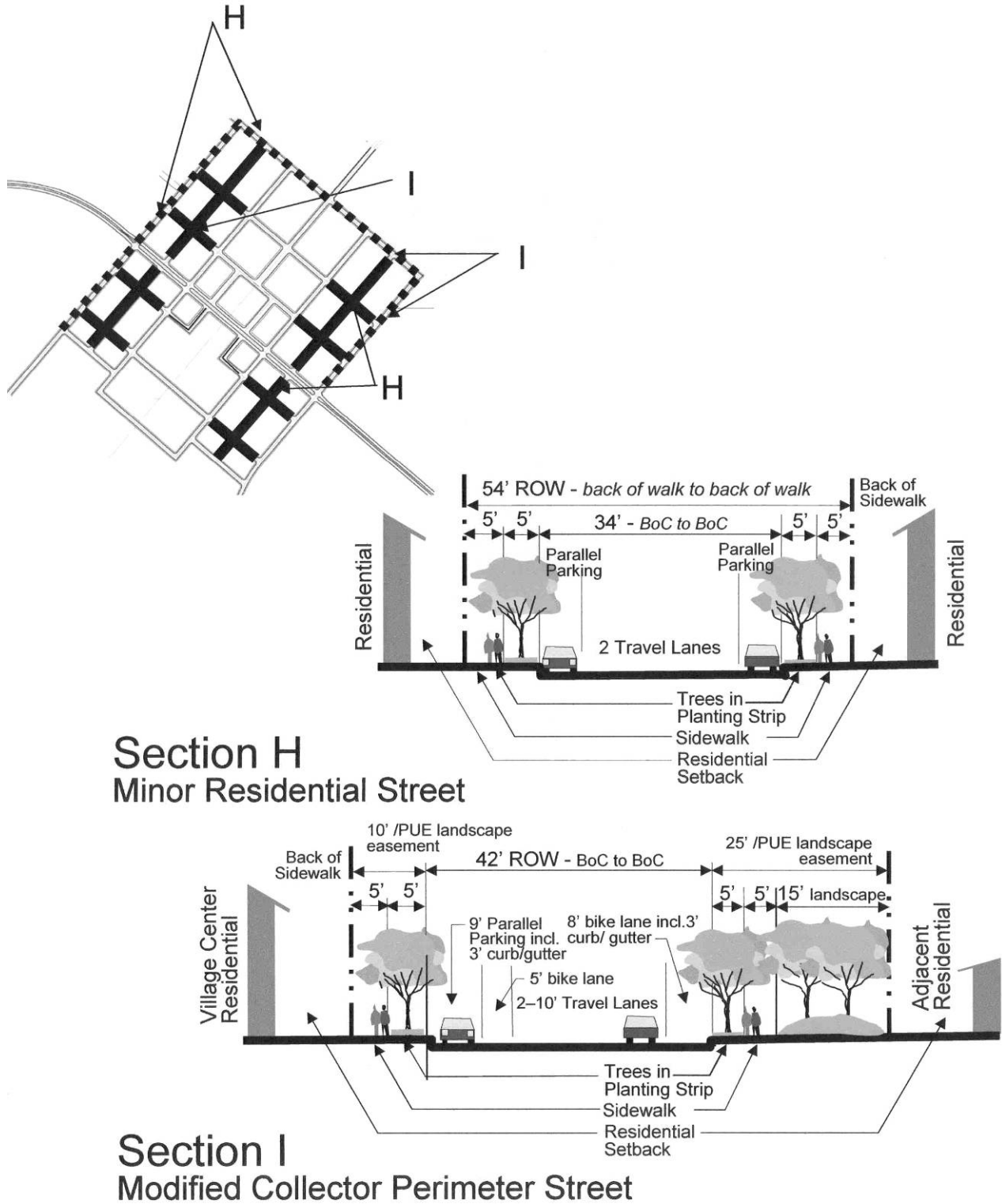


**Section F
Major Residential Street**



**Section G
Residential Entry Street**

Figure 10-22 Village Center Minor Residential and Perimeter Street Sections



Key components of the Village Center roadway system include:

Pleasant Grove Boulevard (Figure 10-18) is a 4-lane arterial that provides access through the WRSP connecting to other portions of the City. This roadway passes through and provides a high degree of exposure to the Village Center. As Pleasant Grove Boulevard enters the Village Center there is a noticeable change in the streetscape environment. The landscape median more than doubles in width and typical suburban landscape corridors and soundwalls are replaced by planter strips and front-forward building architecture.

- **Gateways and Enhanced Pedestrian Crossing** – Gateways are provided at both ends of the Village Center along Pleasant Grove Boulevard. The intent is to create a sense of arrival and calm traffic flows. In addition, an enhanced pedestrian crossing will be provided in order to facilitate the safe and efficient crossing of pedestrians and cyclists across Pleasant Grove Boulevard between the north and south sides of the Village Center, and provide the bikeway link between Class I paths north and south of the Village Center. The enhanced pedestrian crossing will be located at the signalized intersection of Pleasant Grove Boulevard and Market Street, and shall incorporate the following elements:
 - Extra wide street crossing;
 - Change of texture/color within crossing (stamped concrete);
 - Elevated design elements (such as planters or other elements) at the corners of the crossing; and
 - Use of bollards at corners and within median to announce crossing.

Figures 10-23 and 10-24 provide an illustrative and plan view of the gateways and enhanced pedestrian crossing. Additional detail is included in the WRSP Design Guidelines (Section 12).

- **Turning movements** – Traffic signals are located along Pleasant Grove Boulevard at the east and west ends of the Village Center to control traffic and access through the area. Separate turn lanes shall be provided for all turning movements of all legs of the signalized intersections with the exception of separate right turn lanes which may be eliminated along Pleasant Grove Boulevard within the Village Center. Vehicle turning movements are allowed and pedestrian crossings are encouraged at intersections flanking the commercial and Village Green uses. Figure 10-25 provides the turning movement plan for Pleasant Grove Boulevard within the Village Center.

{Figure 10-23 removed September 6, 2017 by Resolution 17-393}

Figure 10-24: Village Center Gateway and Enhanced Pedestrian Crossing-Plan View

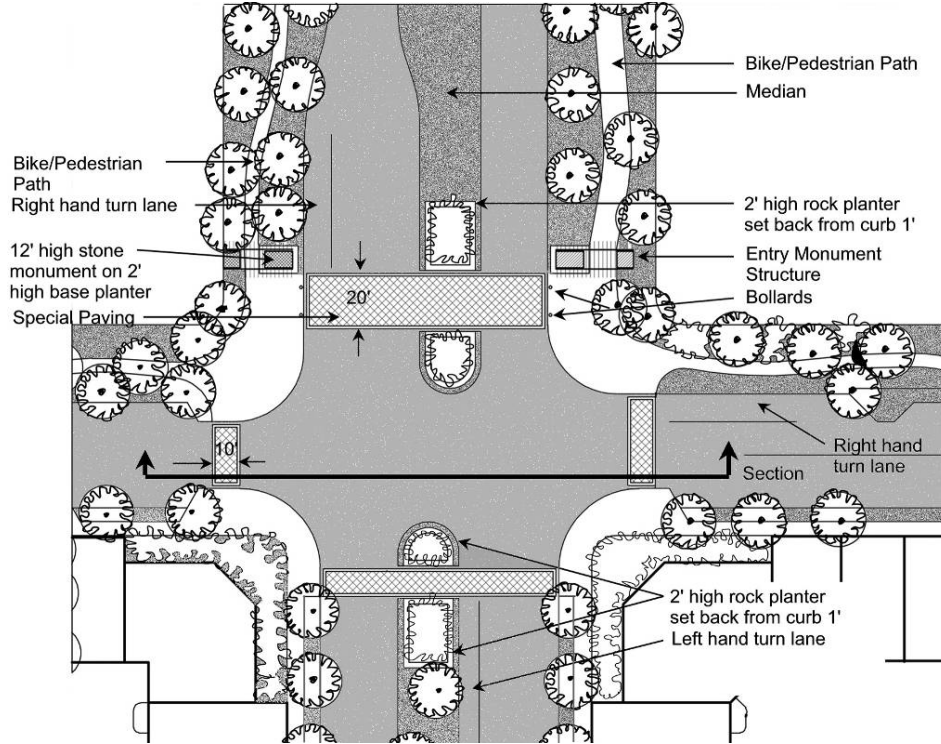
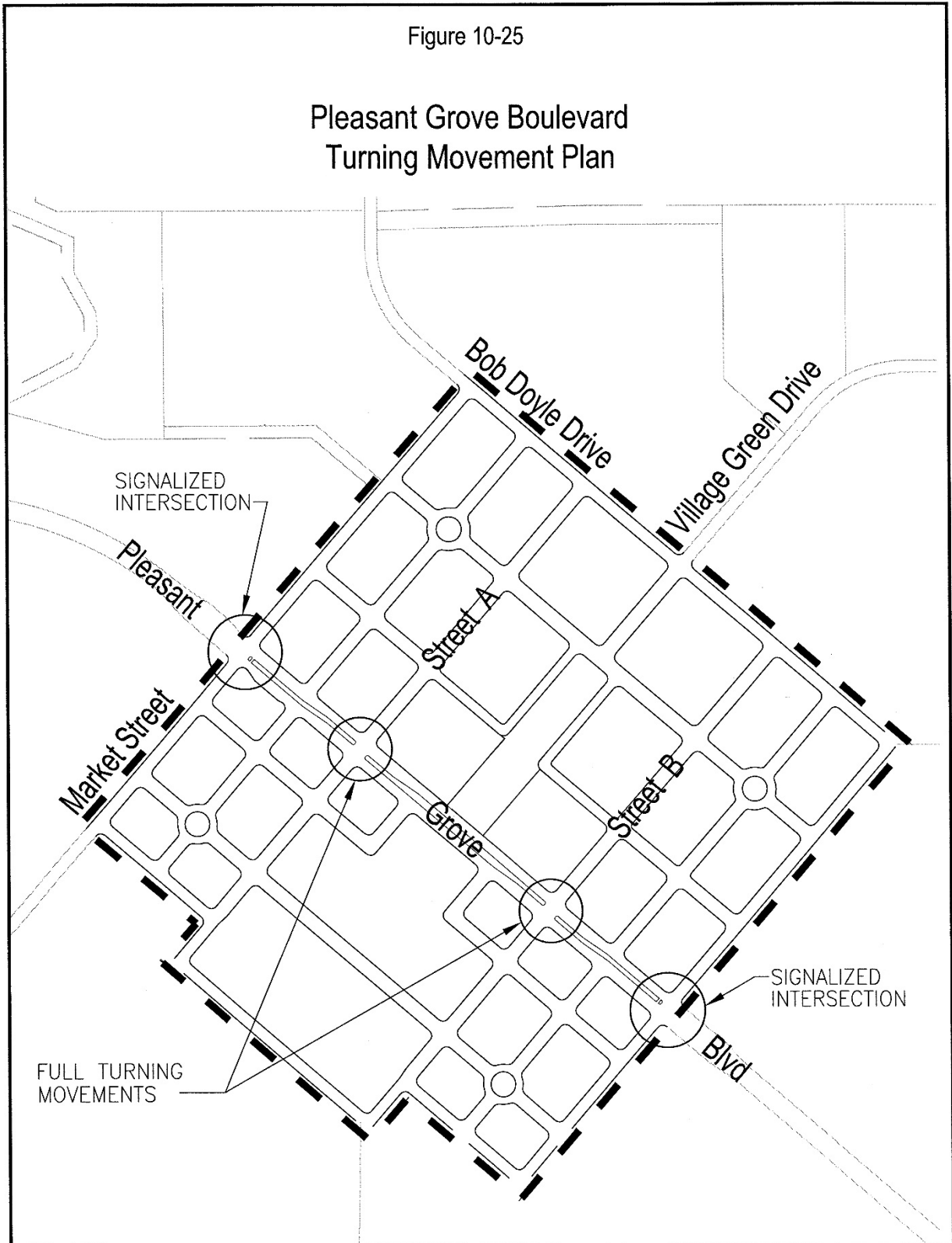


Figure 10-25: Pleasant Grove Boulevard Turning Movement Plan



Commercial Roadways Pleasant Grove Boulevard, Village Center Drive, and Village Plaza Drive are the focus of commercial and social activity within the Village Center. These streets include wide sidewalks, significant tree plantings and other elements to enhance the pedestrian environment. Commercial development is oriented towards surrounding streets and wide sidewalks are provided to give pedestrians options to move between the commercial parcels and the central Village Green.



Residential Roadways are 2-lane streets that provide access to Village Center housing. Residential streets include 5-foot to 6-foot wide planter strips between the curb and sidewalk for tree plantings and other landscaping. Housing faces onto the streets and is encouraged to include entries, porches and other features promoting activity. Alley access may be provided to Medium Density Residential (MDR) small-lot parcels to allow for rear loaded garages and services. Direct auto access from street to garage is prohibited along key Village Center roadways to minimize disruption to on-street parking opportunities (see Figure 10-6).

Four small privately maintained circular greens or roundabouts are located within the intersections of residential streets in the four quadrants of the Village Center (see Figure 10-17 for locations). The circular greens are landscaped elements that interrupt the visual linearity of the roadway system, provide points of orientation, and add to the green landscape quality of the Village Center. Each circular green will be landscaped to assist in orienting pedestrians and motorists within the Village Center. Circular greens are part of the landscaped right-of-way.



Circular Green

□ INTERFACE WITH ADJACENT DEVELOPMENT

Development on the perimeter of the Village Center consists primarily of Low Density Residential (LDR) uses. These uses are encouraged to front onto the Village Center. Where uses do not front onto the Village Center, typical 25-foot wide landscape corridors will be provided. To ensure connection between uses, adjacent development shall provide either direct vehicular or pedestrian connections to Village Center roadways. Pedestrian connections shall consist of an open cul-de-sac with a landscaped pedestrian path and crosswalks. See Village Center/Adjacent Development Interface, Figure 10-26, and WRSP Design Guidelines (Section 12) for additional detail.

□ PEDESTRIAN AND BIKEWAY NETWORK

As previously noted, the Village Center has been designated as a Pedestrian District. It is intended to be a pedestrian friendly-walkable community that minimizes barriers, emphasizes the pedestrian over the automobile, and implements the City's General Plan pedestrian level of service policy. Numerous elements are included to achieve this intent from the basic form and mix of uses, grid circulation pattern, wide sidewalks, dense tree canopy, pedestrian scale lighting and required connections from adjacent uses.

The Village Center will link to the WRSP and City-wide bikeway system through Class II and III bikeways along connecting roadways. In addition, a paseo (Parcel W-85) with a Class I bikeway terminates at the western edge of the Village Center. This Paseo links to the larger WRSP open space and Class I bike trail system (see Circulation Plan, Section 7).

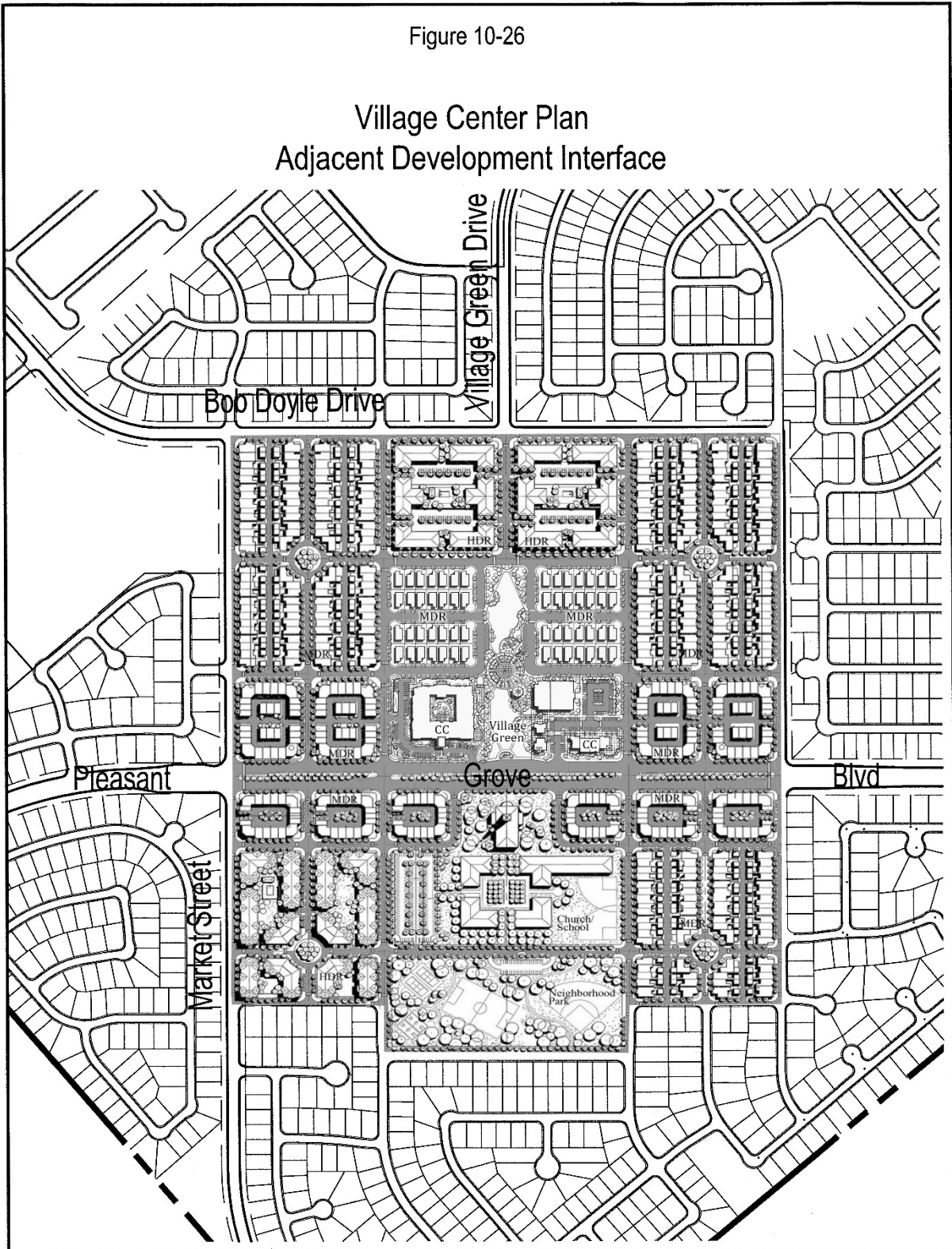
□ PUBLIC TRANSIT

The Village Center includes residential densities considerably higher than a typical suburban setting, averaging approximately 12 dwelling units per acre. This higher density will help to support the use of transit as an alternative to the automobile. Transit stops, with enhanced shelters, are located on either side of Pleasant Grove Boulevard adjacent to the Village Green. Transit service will be provided to the WRSP as demand warrants and funds are available (see Circulation Plan, Section 7).

10.6 PUBLIC SERVICES PLAN

Public Services in the Village Center include those services provided to the rest of the WRSP. All services will generally be provided as presented in Section 8 of the Specific Plan.

Figure 10-26: Village Center/Adjacent Development Interface



□ PARKS & RECREATION

Distinct to the Village Center are two City park sites, the Village Green and a traditional neighborhood park. Totalling approximately 12.69 acres, the park facilities provide for both social and recreational opportunities and help define the character of the Village Center.

Village Green (Parcel W-54) is a unique 3.71 -acre urban park forming the visual focus of the Village Center. The Village Green consists of grass with a symmetrical set of centrally oriented walkways. In the center is an activated gathering space that can be a venue for community events. Large trees are located along the perimeter to provide shade and define the central community space. The Village Green is designated to provide passive recreation and as a gathering place for the community. Besides its passive recreation value and visual presence, the Village Green is designed to accommodate a variety of community activities such as a farmer's market, arts and crafts shows, celebrations, and performance arts that will attract the community to the Village Center. The Village Green will be owned and operated by the City of Roseville. The park preserves the north/south visual corridor which connects the church and neighborhood park site (W-53). A conceptual plan for the Village Green is included as Figure 10-27.

Neighborhood Park (Parcel W-53) is a 8.98-acre (net) active recreation facility. The park will include soccer and baseball fields, hard courts, a playground, picnic area and parking lot. Facility planning for this park should also strongly consider adding a Community Garden to the park to serve the residential uses in the Village Center. The neighborhood park will serve the recreational needs of the Village Center and adjacent residential area. This facility will be dedicated to and maintained by the City of Roseville. A conceptual plan for the Village Center Neighborhood Park is included as Figure 10-28.



Figure 10-27: Village Green Conceptual Plan

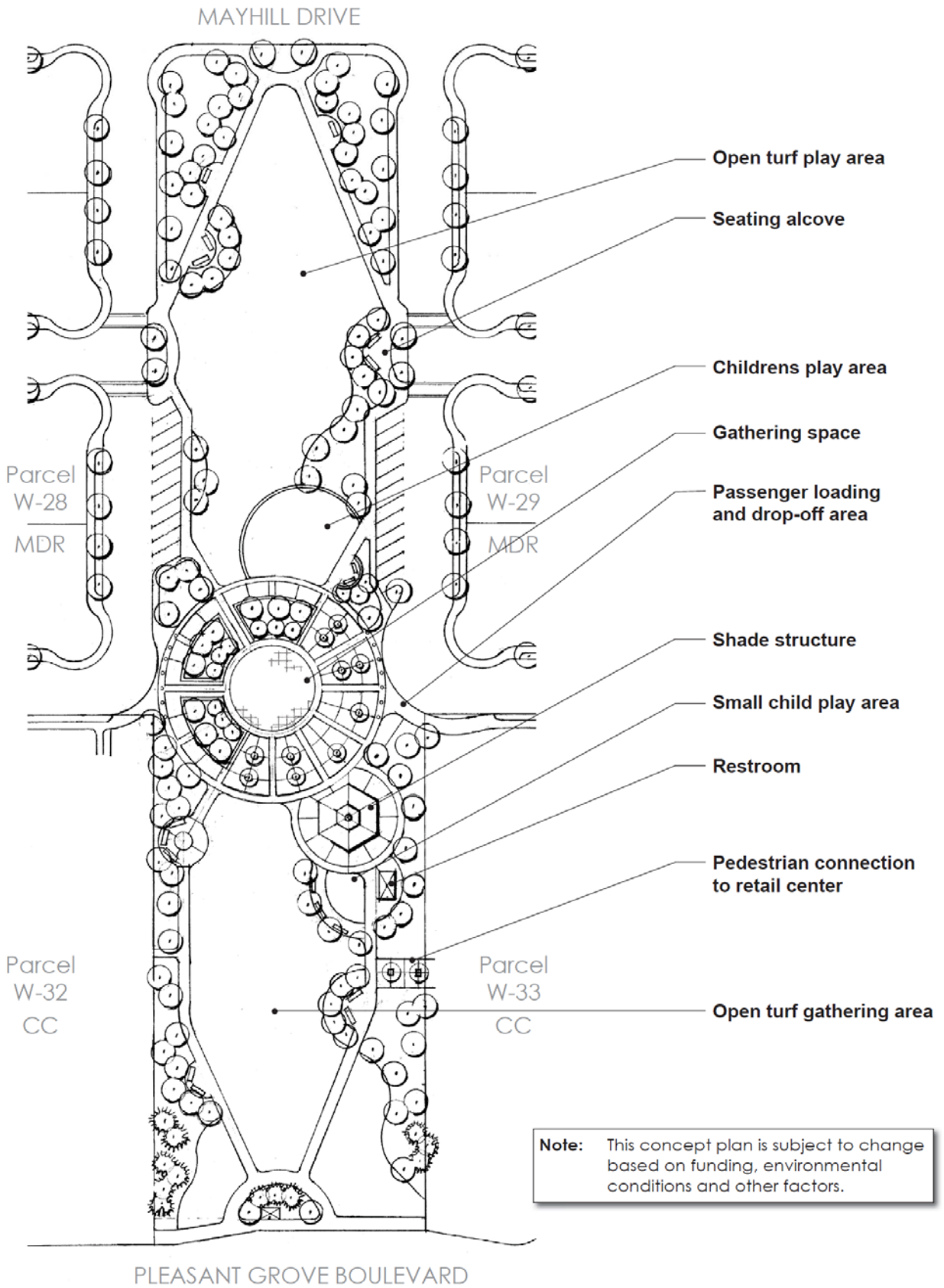
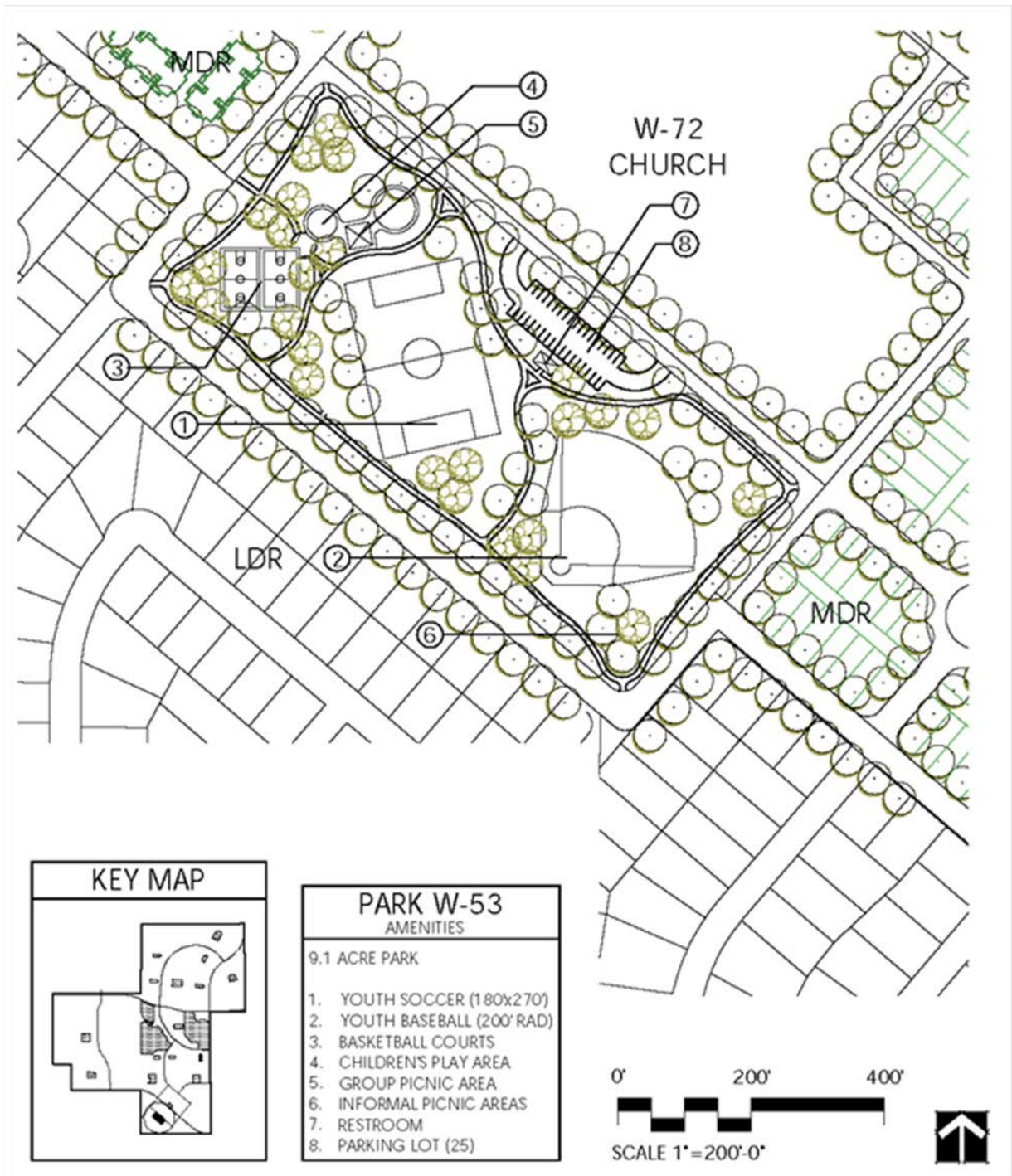


Figure 10-28: Village Center Neighborhood Park Conceptual Plan



Note: Dwelling units and/or acres referenced in this figure are not regularly updated. Refer to Table 4-2 for the most up to date unit and acre information.

10.7 UTILITIES PLAN

Utilities provided to the Village Center include water, wastewater, recycled water, drainage and flood control, electric service, natural gas, communications and solid waste disposal. All services will generally be provided as presented in Section 9 of the Specific Plan.

☐ UTILITY EASEMENTS

MDR parcels within the Village Center may include public alley access. Utility easements may be located within either the adjacent residential streets or alleys.

10.8 IMPLEMENTATION & ADMINISTRATION

The Village Center will be subject to the Implementation and Administration provisions presented in Section 11 of the Specific Plan.

The Village Center is within Phase 2 of the WRSP. To help facilitate its development, Phase 2 supplies the backbone roadway infrastructure necessary to service each large-lot Village Center parcel. Phase 2 backbone road improvements within the Village Center are reflected on Figure 10-29. Given these Phase 2 improvements, no additional internal phasing of the Village Center is required.

Development of the Village Center will necessitate the construction of additional parcel specific improvements. These parcel specific improvements are detailed in the Westpark development agreement.

Figure 10-29: Village Center Roadway Improvements in WRSP Phase 2

